

# FAIRVIEW HILLS REFINEMENT PLAN



Prepared for

**SIMPSON HILLS, LLC**

Revised July 31, 2012



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3. May 6, 2009 AAR Cultural Resources Survey summary
4. June 8, 2009 AAR Archaeological Survey
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6. July 1, 2009 AAR Archaeological Monitoring Results Summary
7. September 9, 2010 - Applied Archeology Research Cultural Resources Investigation
8. January 12, 2012 - GeoPacific Geological Hazard Assessment Report
9. February 3, 2012 - GeoPacific Geotechnical Investigation
10. February 7, 2012 - Kittelson & Associates Trip Generation Analysis
11. March 27, 2012 – AKS Reduction in Water Usage Letter
12. May 8, 2012 – GeoPacific Infiltration Testing Letter
13. May 9, 2012 – AKS LEED Information Letter
14. May 11, 2012 – GeoPacific Response to Review Letters
15. May 14, 2012 – Foundation Engineering, Inc. Review of Site Infiltration Data
16. July 15-16, 2012 – Earth Advantage vs. LEED information



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# FAIRVIEW HILLS REFINEMENT PLAN

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## EXECUTIVE SUMMARY

The Fairview Hills Community will be a new kind of neighborhood for the City of Salem. It will look and feel different from other City neighborhoods. Streets will be narrow, oriented towards the pedestrian and feature short walkable blocks. Innovative buildings will be nestled between and face wide green open space corridors connected with an extensive network of sidewalks, trails, and pathways. Diverse housing and building types will be established to encourage community interaction. Due to expansive open space provisions, the neighborhood is able to achieve needed urban level densities while preserving existing natural resources. Simply put, there is nothing like the Fairview Hills Community that exists in Salem today.

The roots of the Fairview Hills Refinement Plan can be traced to the Fairview Master Plan, a unique document created to serve as the vision for the 275 acre former Fairview Training Center property. The Fairview Hills Refinement Plan application implements the Fairview Master Plan and sets a new standard for the City of Salem's Refinement Plan process. The Fairview Hills Refinement Plan is the most comprehensive Refinement Plan that has been submitted to the City to date. Establishing the Fairview Hills Community is the lynchpin to activate this important and unique area that has remained dormant for years.

The Fairview Hills Refinement Plan is the culmination of many months of planning, working closely with City of Salem staff, and dozens of meetings with members and representatives from the community including the Morningside Neighborhood Association, Sustainable Fairview Associates, and the Pringle Creek Community. The essential components of the Fairview Hills Refinement Plan are as follows:

- **An Interconnected and Multi-Layered Transportation System.** The Fairview Hills Community will feature walkable blocks of public and private streets interwoven with off-street pathways / pedestrian trails that traverse through wide green vegetated corridors, around native white oaks and natural wetlands.
- **Engaging Streetscapes.** Streets will be narrow, pedestrian friendly and lined with street trees, vegetated swales, and multi-family homes with inviting front porches and entryways built to a human scale.
- **Wide Variety of Interesting and Attractive Buildings / Housing Types.** Apartment homes, townhomes, duplexes, and carriage houses are dispersed throughout the community, connected by way of an extensive pedestrian network, and oriented towards streets and green corridors.
- **Innovative Green Buildings.** The community will achieve Earth Advantage Multi-Family Certification. Earth Advantage is a recognized leader in the sustainable building industry and achieving Earth Advantage Certification represents a strong commitment to building sustainable green buildings.
- **A Low Impact Stormwater System.** Green infrastructure techniques and stormwater management measures tailored to specifically honor the site's historic drainage patterns will be employed. This multi-faceted approach includes acres of preserved open space, street-side bioswales, vegetated filter strips, infiltration planters, areas of permeable pavement, and an extended dry basin.
- **Upgraded Community Access – Improved Reed Road SE.** Reed Road SE will be improved to 34 feet wide with the first phase of the Fairview Hills project. This will provide a safe means of access for the community and for those traveling nearby.



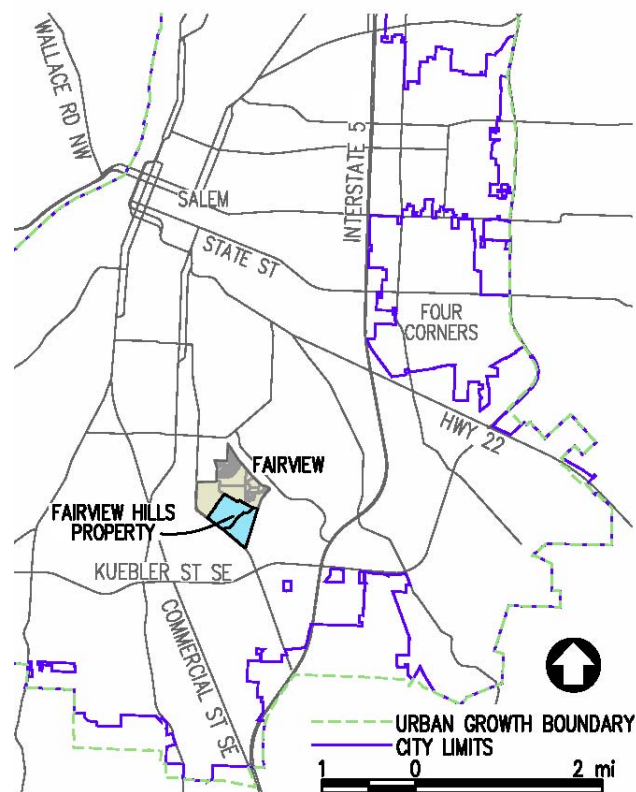


Figure 1: Fairview Hills Refinement Plan

## INTRODUCTION

Simpson Hills, LLC owns approximately 103.8± acres of the former Fairview Training Center property and has prepared this Refinement Plan to the adopted Fairview Master Plan to comply with Salem Revised Code (SRC) Chapter 143C.090. Simpson Hills, LLC has elected to prepare a Refinement Plan for the northern portion of the site which contains no less than forty (40) acres as permitted under SRC 143C.100.(c). A separate Refinement Plan will be prepared for the remainder of the property in the future. This document has been prepared based upon the Fairview Master Plan, applicable portions of the City of Salem Revised Code, the City of Salem Development Design Handbook, and other relevant regulations. Simpson Hills, LLC looks forward to the opportunity of creating a positive impact on the local environment, surrounding neighborhood, and City of Salem.

The development of the refinement area will integrate a positive mix of pedestrian and vehicular access while at the same time incorporating generous open spaces with natural drainages, wetlands, and trees. This approach creates a balance between practical development and this natural setting in the South Salem hills. Simpson Hills, LLC is excited about moving forward with this project to provide exciting new housing options and neighborhood commercial opportunities within the local community.



**Figure 2: Vicinity Map**

This Refinement Plan contains many of the desirable elements included in the adopted Fairview Master Plan and is consistent with the Principles of the Plan as illustrated throughout this document and as highlighted by the following:

### **1. Build in Economic and Social Diversity**

- This will be a new kind of multi-family neighborhood for the Willamette Valley with front porches facing streets, multi-use pathways, trail system, open lawn/gathering area, a community plaza, community gardens, localized rain barrels for watering, and extensive and interconnected system of public and private streets with sidewalks, multi-purpose pathways, and trails.
- The community will contain a variety of housing unit types including apartment units (i.e. studios, one / two / and three bedroom), duplexes, townhomes, and carriage houses (dwelling unit(s) over garages). These are mixed throughout the project so as to maximize the opportunities for interaction between different types of people. Each unit type will appeal to one or several potential groups. This level of diversity is needed to complement the single-family detached residential lots that currently exist in the FMU zone.
- The Fairview Hills Refinement Plan provides a mix of land uses permitted in the FMU-MI zone including needed multi-family residential homes and a community-scale office / commercial area.
- Provisions for barrier free (accessibility) adaptability within all the ground floor units, barrier free recreational amenities, and barrier free parking stalls to meet and/or exceed federal and state regulations are planned.
- The anticipated recreational amenities in each development include a community plaza, an exercise area, and exterior gathering areas for celebrations and family functions, including swimming pool, spa, and play areas for children. These amenities will appeal to all age groups and offer diverse recreational opportunities.

### **2. Create a Center**

- The Fairview Hills Refinement Plan features a network of streets, pedestrian ways, and multi-use pathways coalescing toward and connecting all parts of the community with, and emphasizing the role of, the adjacent village center envisioned in the Fairview Master Plan. In addition, the Refinement Plan will include on-site focal points such as a community plaza, community gardens, and a neighborhood-scaled commercial node.
- The Refinement Plan creates a focal point “Main Entry” leading into the community at the Lindburg Road SE and Reed Rd. intersection.
- Natural linkages including “Green Corridors” with multi-use trails are planned to converge into the hollow “palm” located in the center of the former Fairview Training Center site.
- A centrally located community plaza is provided. The plaza allows for formal and



informal gatherings for all people.

- The focal point “Main Entry, “Green Corridor” linkages, plaza, and public street/sidewalk connections included in the Refinement Plan help achieve the Center that is envisioned in the adopted Master Plan.



**Figure 3: Main Entry Concept**

### **3. Re-Use, Retrofit**

- There are no pre-existing structures from the former Fairview Training Center on the site that are suitable for re-use; however, the Refinement Plan incorporates sustainable site development practices by reuse of existing on-site materials such as rock / gravel and materials from previous buildings that have been removed. This green building practice minimizes material import and haul off.

### **4. Create Local Employment**

- A community scaled commercial node is included in the Refinement Plan area. The mix of land uses included in the Refinement Plan is permitted and encouraged by the FMU MI Overlay zone. The proposed commercial node is conveniently located within walking distance of the future multi-family residential areas. When the local population is capable of supporting commercial land uses in the future, community scale office and retail businesses can be established in the commercial node. Together with home occupations, this is supportive of the local employment creation principle established in the adopted Fairview Plan.

## **5. Build Innovative Green Buildings**

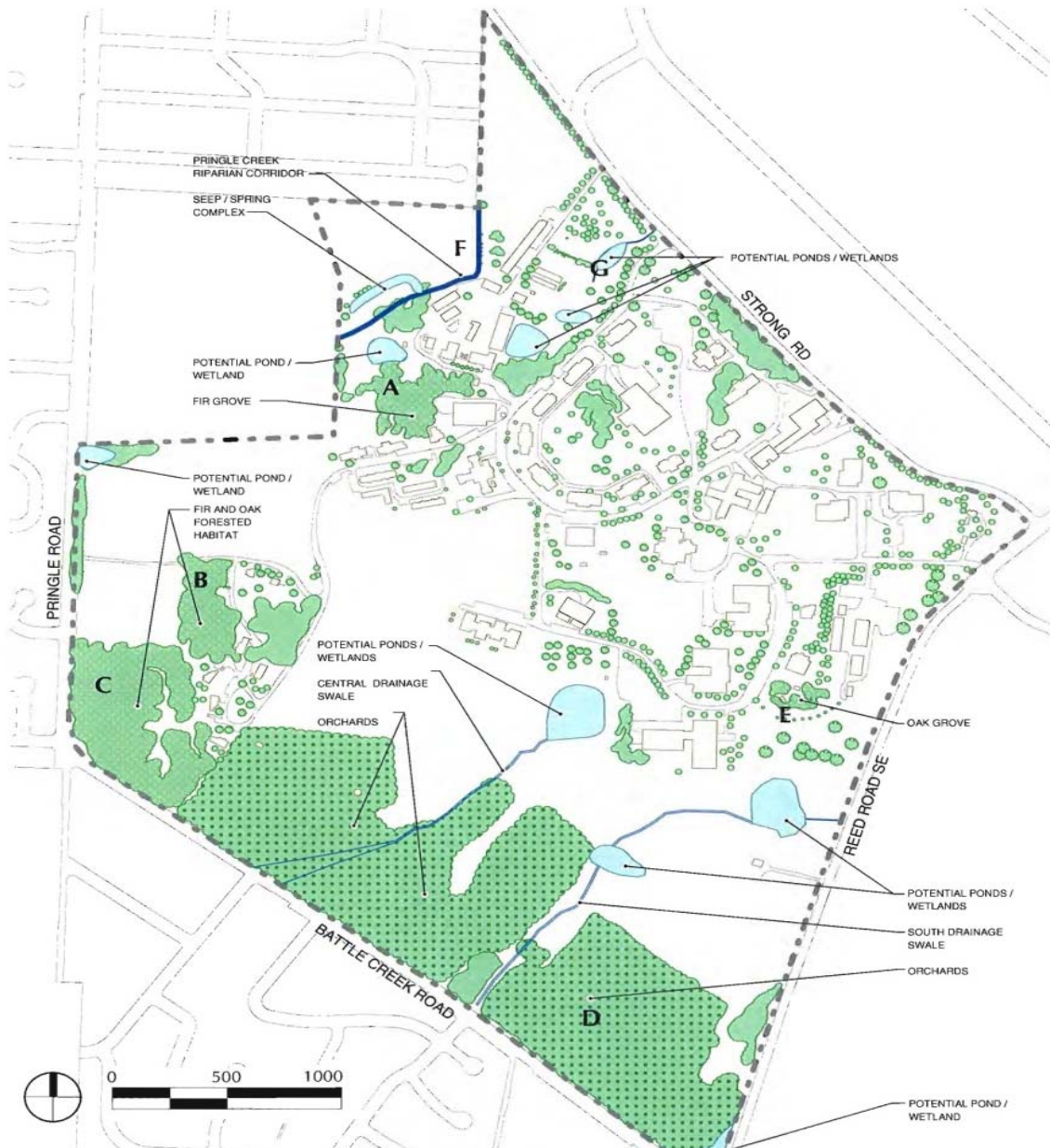
- The community will obtain Earth Advantage® Multifamily certification and green building techniques will be incorporated. Earth Advantage is an industry leader in green building technology. Similar to LEED®, Earth Advantage Multifamily, involves a scoring system that involves achieving points for implementing a variety of green building / development measures within a project. The scoring system includes categories such as Site Planning, Waste Management, Building Envelope and Systems, Heating and Cooling Systems, Infiltration / Air Sealing / Ventilation, Lighting, Appliances, and Water Heating, Indoor Air Quality, Resource, Resource Efficient Building Materials, Water Efficiency and Landscaping, Solar Measures, and Bonus and Innovative New Measures.

## **6. Respect the Landscape**

- The Refinement Plan creates generous open spaces and “Green Corridors”, which are designed to protect the existing natural resources including wetlands and habitat, while also providing for recreational use through the system of proposed multi-use pathways. As envisioned in the adopted Fairview Master Plan, these areas lead to the heart of the former Training Center property.
- The General Landscape Plan demonstrates how green infrastructure (i.e. bioswales, infiltration planters, pervious pavement, etc.) will be incorporated into the project.

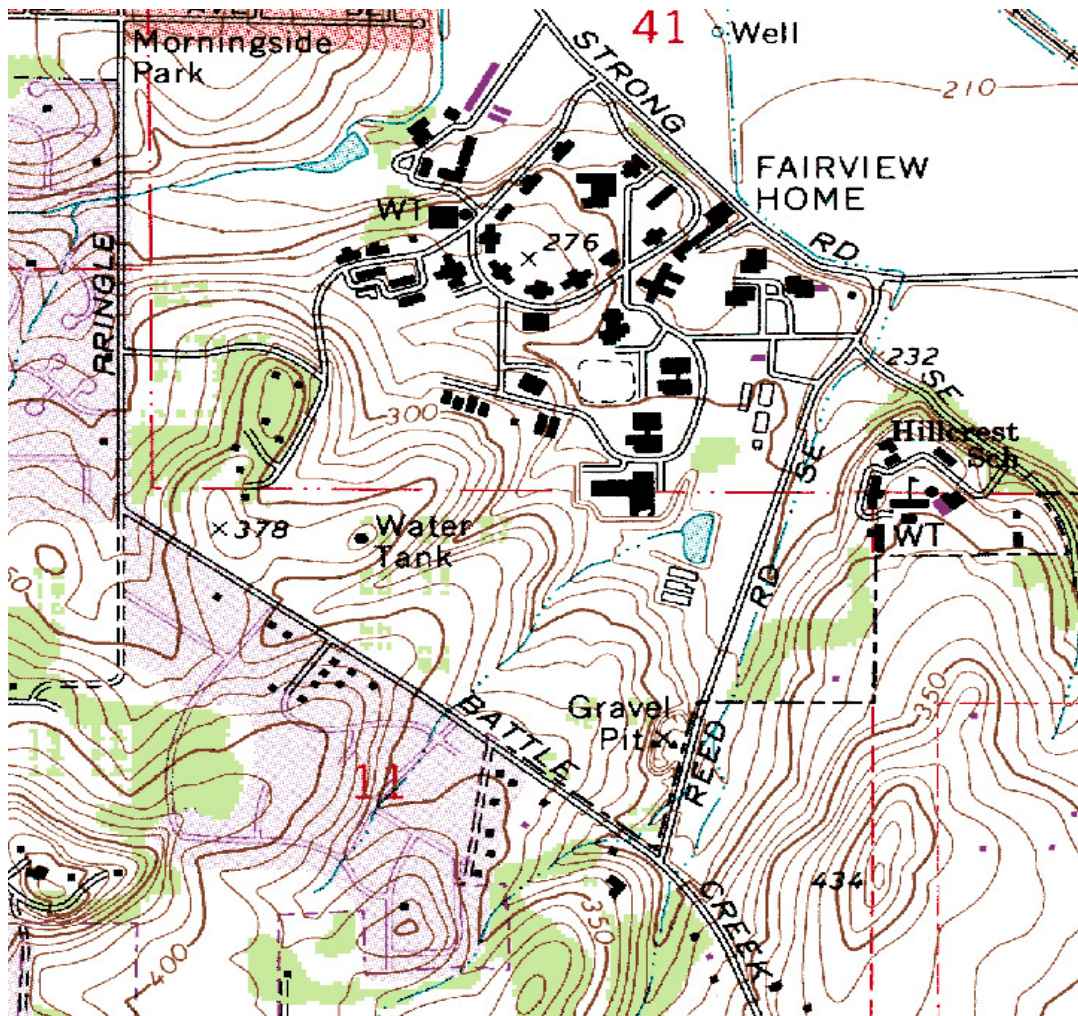
## **7. Zero Impact to the Regional Watershed**

- Zero impact to the Regional Watershed is achieved as follows.
  - The post-development storm water outflow will not exceed the historic pre-developed 5-year design storm runoff from the site.
  - West Middle Fork Pringle Creek will continue to receive pre-development storm water runoff flow rates so that critical creek flows are maintained.
  - Natural surface infiltration will continue to take place over approximately 14.8± acres of open space within the Refinement Plan area.
- Stormwater will continue to flow naturally along the existing natural drainages to naturally cleanse, infiltrate, and recharge the aquifer below as envisioned in the Fairview Master Plan.
- Innovative Low Impact Development Approaches and green infrastructure techniques are featured in the Fairview Hills Refinement Plan. This includes bioswales, vegetated filter strips, infiltration planters, and permeable pavement.
- Stormwater facilities are provided adjacent to the streets as shown on the typical private street cross sections, where possible, will improve water quality, simulate natural drainage conditions, and create roadway aesthetics as envisioned in the Fairview Master Plan.



**Figure 4: Fairview Master Plan Natural Resources Exhibit**

- The Refinement Plan respects the existing drainage ways and honors the historic low-lying areas in the same general location shown on the United States Geological Survey (USGS) Quadrangle Map of the site.



**Figure 5: USGS Quadrangle Map**

- Stormwater quality treatment of runoff from impervious surfaces will be provided for all development within the Refinement Plan area (41.1± acres) and the remaining 63.5± acres of the Fairview Hills property located outside of the Refinement Plan area in conformance with the applicable policies and procedures of Clean Water Services of Washington County (CWS) Design and Construction Standards (R&O 07-20) as of the date of this Refinement Plan. This will result in an improvement in the water quality of Pringle Creek as envisioned in the Master Plan.
- Innovative land development practices are being incorporated by implementing CWS stormwater quality treatment standards when no such standards exist in the City of Salem which is another sustainable practice that sets this project apart from other communities in the area.
- CWS is recognized as a leader in Low Impact Development Approaches (LIDA) for stormwater management and by adopting the above referenced CWS stormwater quality

treatment standards, the project is incorporating the best existing ideas on design and construction of the Refinement Plan area infrastructure as desired by the Fairview Master Plan.

## **8. Layer the Systems**

- Consistent with this principle, residential and commercial land uses are intermixed in the proposed Fairview Hills Refinement Plan. As previously described, “Green Corridors” are integrated into the Refinement Plan. The mix of land uses and green corridors represent a balanced approach to development of the site, one which includes proper green stormwater management measures, promotes wetland and habitat protection, while allowing for appropriate recreational opportunities.
- The stormwater system is integrated into the transportation and pedestrian system with streetside swales and green corridors.

## **9. Close the Cycle of Energy and Material Flows**

- The “Green Corridors” together with the proposed stormwater quality best management practices (BMPs) and attenuation system allow for on-site infiltration to the extent possible, given soil capabilities.
- Multi-family homes can have higher rates of recycling with lower volumes of refuse when appropriate recycling areas and refuse compactors are provided. Centrally located refuse compactor systems will reduce the volume of refuse. Covered recycling areas conveniently located adjacent to the compactor will be also be provided.
- Use of partially recycled content building materials is included for re-use as gravel and within road beds.

## **10. Green Corridors for People and Other Living Things**

- As described herein and as illustrated in the figures found throughout this Refinement Plan, the “Green Corridors” included in the adopted Fairview Master Plan are proposed in this application. These corridors include the complex layering of habitat and wetland areas, drainage way preservation, passive open space, and appropriate recreational use allowing for pedestrian / bicycle mobility that was envisioned in the Master Plan.

## **11. Transit Close at Hand**

- The site is within ¼ mile of existing transit services. As illustrated throughout this Refinement Plan document, this system includes multi-use paths which are proposed to serve as a connection to Transit Service on Battle Creek Road SE. In addition, the multi-family projects include a convenient system of pedestrian connections that allow for resident access to the surrounding public street system. A transit plan is included as Figures 18 and 19 showing the location of existing and potential future transit lines per the Fairview Master Plan.



## **12. An Interconnected Street System**

- The Refinement Plan includes an interconnected network of public and private streets that integrates with the established street system and promotes connectivity and access to and throughout the site. The streets blend follow existing on-site topography / contours to the extent possible and connect to off-site properties. As envisioned in the Master Plan, the shadow plan shows the high degree of connectivity that is proposed within the Refinement Plan area and that is anticipated for the surrounding area.

## **13. Walk Every Day**

- As illustrated by the figures contained in this document, the Fairview Hills Refinement Plan includes a multi-layered system of sidewalks, multi-use pathways, and pedestrian connections. This ensures a very walkable community as envisioned in the Master Plan.
- Sidewalks are included on both sides of all streets, pedestrian pathways throughout open spaces, a network of trails and multi-use pathways are proposed.

In addition to being consistent with the Principles of the adopted Fairview Master Plan, the Fairview Hills Refinement Plan is also compatible and consistent with the Salem Area Comprehensive Plan. The Refinement Plan accomplishes this by:

- Allowing housing opportunities that support affordable housing for a diverse range of incomes mixed with a complementary neighborhood scale commercial / office use;
- Developing the property in a manner that is convenient, safe, and attractive to pedestrians;
- Providing a combination of public and private streets that allow for pedestrian and bicycle circulation and access to mass transit on the existing Battle Creek Road and Fairview Industrial Drive routes;
- Facilitating the efficient use of land by encouraging compact high-density development providing approximately 13 to 14 dwelling units per acre that is supportive of mass transit;
- Large open space areas that protect significant trees, wetlands, and provide for recreational opportunities;
- Compatibility with adjoining uses such as the Fairview Refinement Plan II through the collaborative development of Lindburg Road SE;
- Implementation of necessary infrastructure and services such as transportation (as described above), sanitary sewer, stormwater management, and water services, etc;
- Encourages high densities and sustainable site development practices and efficient use of land as describe above, as well as utilization of construction techniques and management practices that will control soil erosion and stream sedimentation;
- Providing for a neighborhood scale mix of office/commercial that will enhance and



- encourage economic opportunities that support the local economy;
- Including energy efficient multi-family construction that meets modern energy codes with comparatively reduced energy consumption needs;
  - Improving air and water quality through preservation of significant trees and inclusion of generous open space areas; and a substantial stormwater attenuation area;
  - Complies with the requirements of the FMU zone and other applicable City standards except where refinements to said standards are established herein; and
  - Offers diverse recreational opportunities for residents such as children's play areas, recreation centers, pools, spas, community gardens, community plaza, multi-use paths, etc.

## **ILLUSTRATIVE SITE PLAN (1)**

The Fairview Hills Refinement Plan includes approximately 41.1± acres of the 103.8± acre property currently owned by Simpson Hills, LLC. As shown below, the Refinement Plan area is generally located in the northern portion of the site, bounded by Lindburg Road SE (future) to the north, Reed Road SE to the east, 2nd Street (future) to the south, and the Sustainable Fairview Associates (SFA) property to the north and west. At the request of City of Salem staff, a subdivision application is also being submitted for the property. Although that application is being submitted separately from this Refinement Plan, the proposed subdivision lots are included below for illustrative purposes.



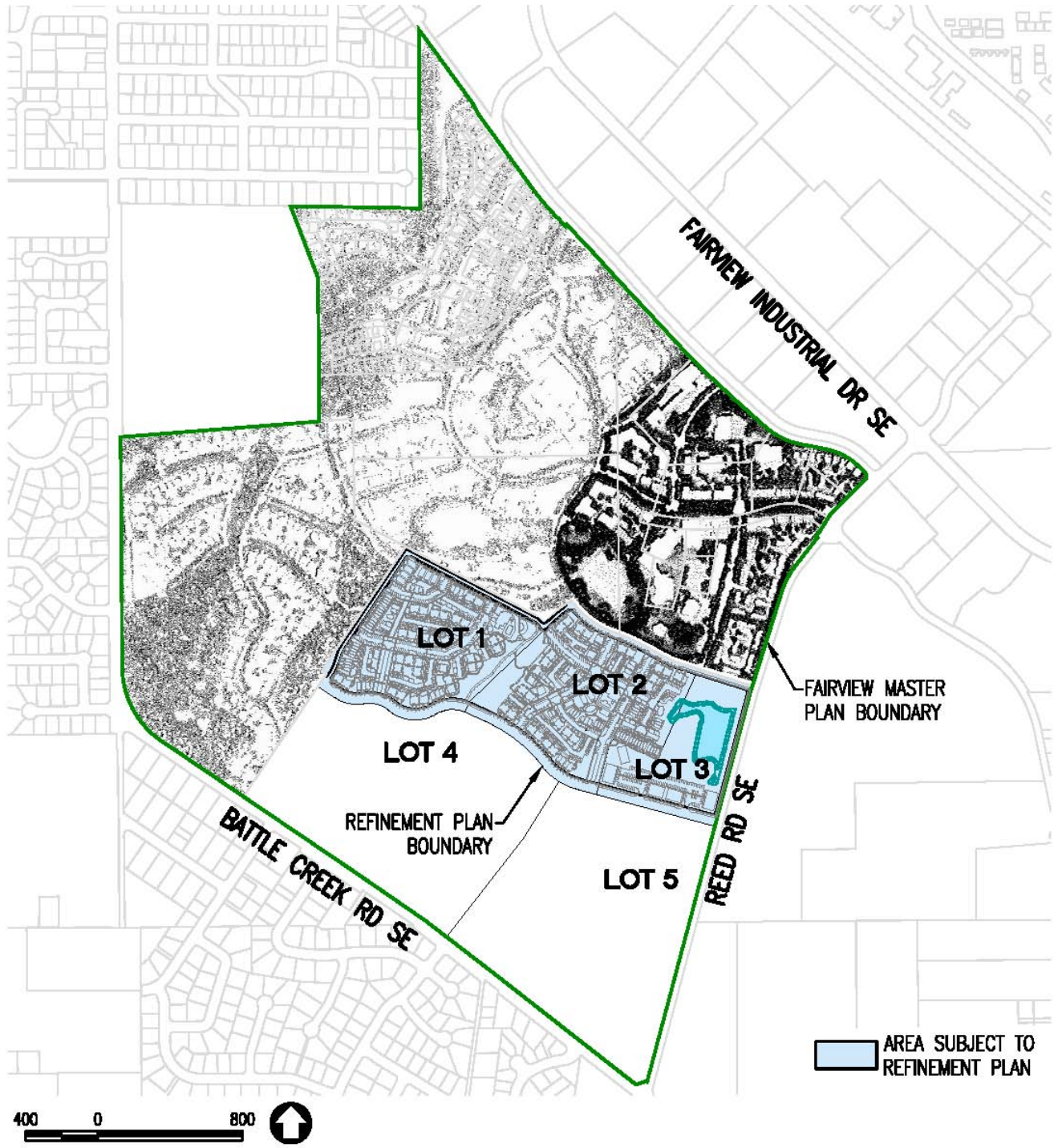
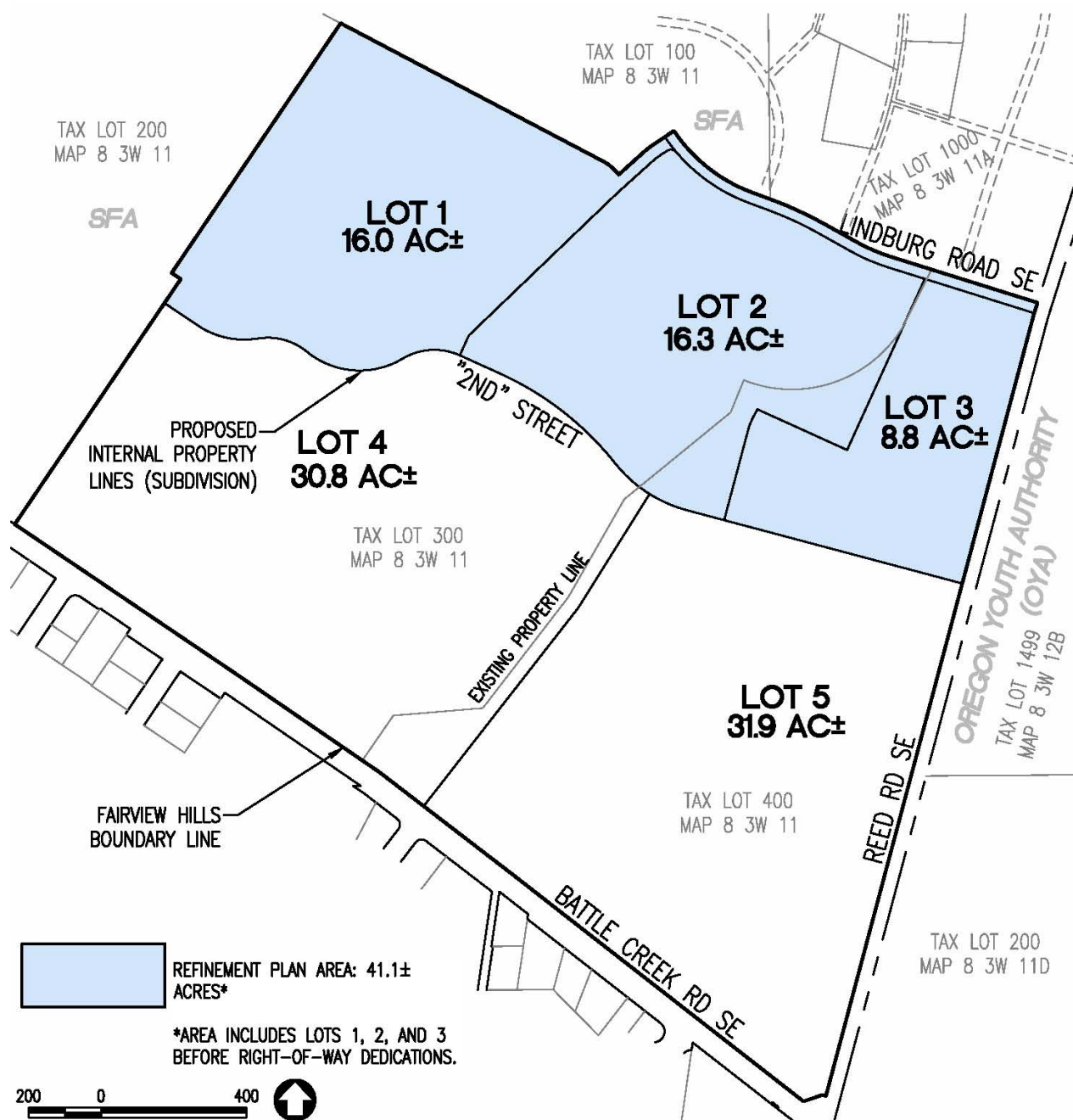


Figure 6: Refinement Plan Area in the Fairview Master Plan context



**Figure 7: Refinement Plan Area**



**Figure 8: Illustrative Conceptual Site Plan with key site features**



**Figure 9: Conceptual Multi-Family Development Rendering**



**Figure 10: Conceptual Multi-Family Development Rendering view from Lindburg Rd. SE**

Conscientious advance planning efforts have been made by the Project Team to ensure that the Refinement Plan will integrate with surrounding land uses located in the Morningside Neighborhood area. This includes the existing Pringle Creek Community, other approved Refinement Plans (Fairview Refinement Plan II), remaining portions of the Fairview Hills property, and other undeveloped portions of the Fairview Master Plan.

The development of the Refinement Plan area will occur over time in phases as outlined in Sections (11) and (13).

## **GENERAL ALLOCATION AND IDENTIFICATION OF MAJOR PROPOSED LAND USES (2)**

The general allocation of major land uses follows the adopted Fairview Master Plan and Figure 143C-2 of the SRC, with only one zone (MI) present in this Refinement Plan area as shown on Figure 11 below.

- Mixed-Intensity (MI)
  - Residential uses and neighborhood commercial, employment, and civic uses.
  - 7 to 35 dwelling units per gross acre for all residential development in the FMU zone.
  - No building used exclusively for a non-residential use shall have a building footprint greater than 6,000 square feet for non-residential development.

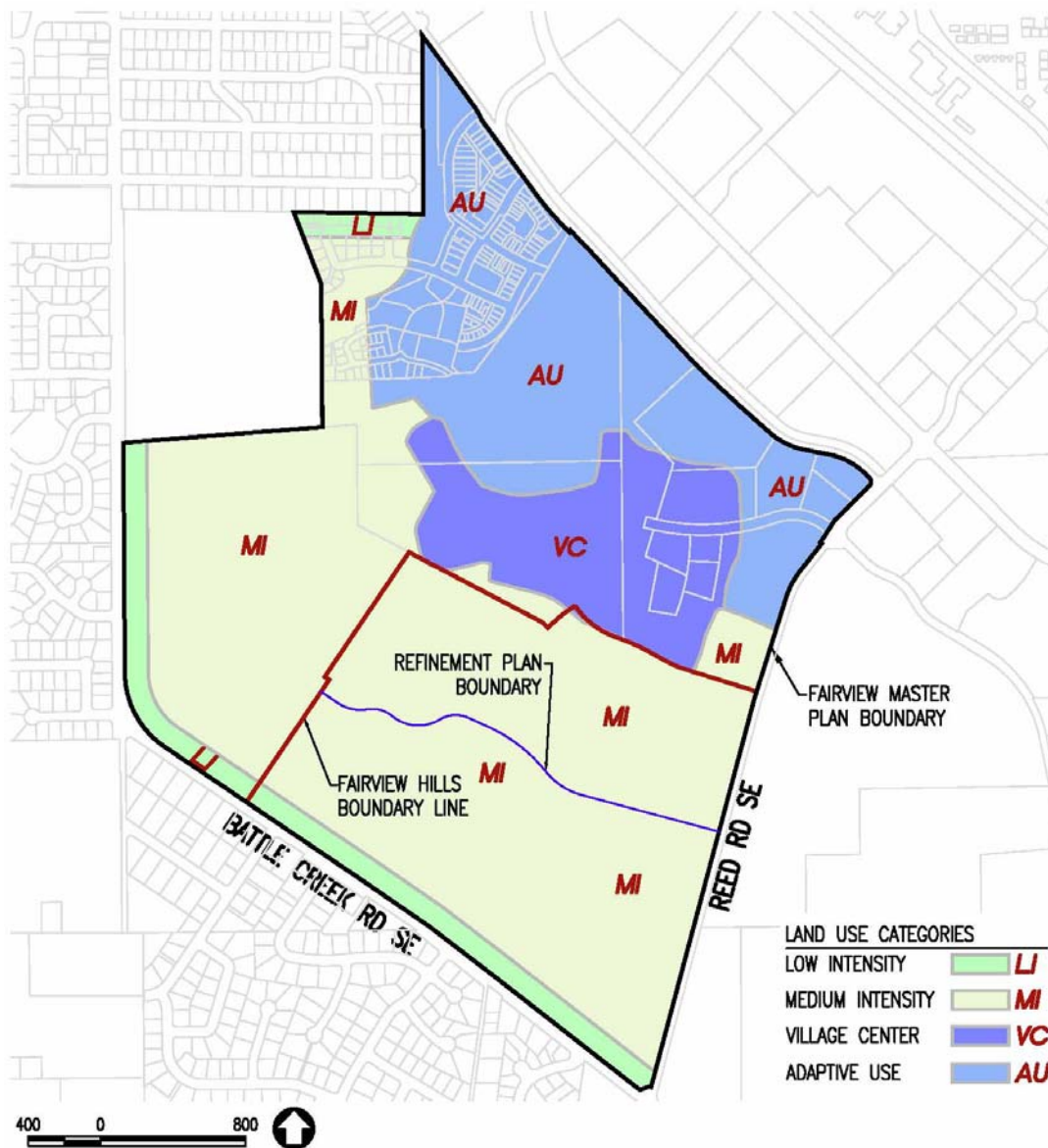
The Refinement Plan area also includes several open space areas comprised of two natural drainages, wetlands, trees, and stormwater quality/detention area.

The May 2011 Salem-Keizer Housing Needs Analysis 2012 to 2032 prepared for the Mid-Willamette Valley Council of Governments states that “Salem has a deficit of multifamily land” and needs approximately “3,283 dwelling units”. The Refinement Plan density and proposed uses outlined below help to reduce this multifamily deficit.

- Apartments / Multi-family Residential
  - Lot 1 (16.0± gross acres): 217± units = 13.6± dwelling units per gross acre
  - Lot 2 (16.3± gross acres): 218± units = 13.4± dwelling units per gross acre
- Neighborhood Commercial, Employment and/or Civic Uses
  - Lot 3 (8.8± gross acres): 24,000± square feet of buildings including a mix of parking, open space, natural areas, and a stormwater facility.

The lot gross acreage shown is prior to any dedication of public right-of-ways. The above number of units and commercial building square footage are approximate as the final numbers may vary depending on market demand.

This provides an overall residential use density of 13.5± units/acre with approximately 435± residential units on 32.3± gross acres of land; and no commercial building footprint exceeding 6,000 square feet in conformance with Table 143C-1 “non-residential” uses. As illustrated above, the 41.1± acre Fairview Hills Refinement Plan site, which represents approximately 14.9% of the FMU land area, exceeds its assumed minimum contribution of 251 dwelling units. Therefore, the refinement plan is consistent with achieving the overall residential density goals of the Fairview Plan.



**Figure 11: FMU Overlay Zones**

## NAME, LOCATION AND EXTENT OF EXISTING OR PROPOSED MAJOR STREETS (3)

The refinement area street network consists of a series of public and private streets accessing the principal collector street (Lindburg Road SE), approved as part of the Fairview Refinement Plan II, and Reed Road SE (minor arterial). "2nd" Street will be dedicated to the public and constructed in conformance with City of Salem local street standards. "1<sup>st</sup>" Street and "A" Street will be dedicated to the public and constructed in conformance with City of Salem standards as modified herein.

Lindburg Road SE will be dedicated to the public and constructed in accordance with the Fairview Refinement Plan II from Reed Road west along the north Fairview Hills property line in a cooperative effort between Simpson Hills, LLC and Sustainable Fairview Associates (SFA).

The private internal street network shown within the Refinement Area will be constructed as required with development (building permit) for each lot.

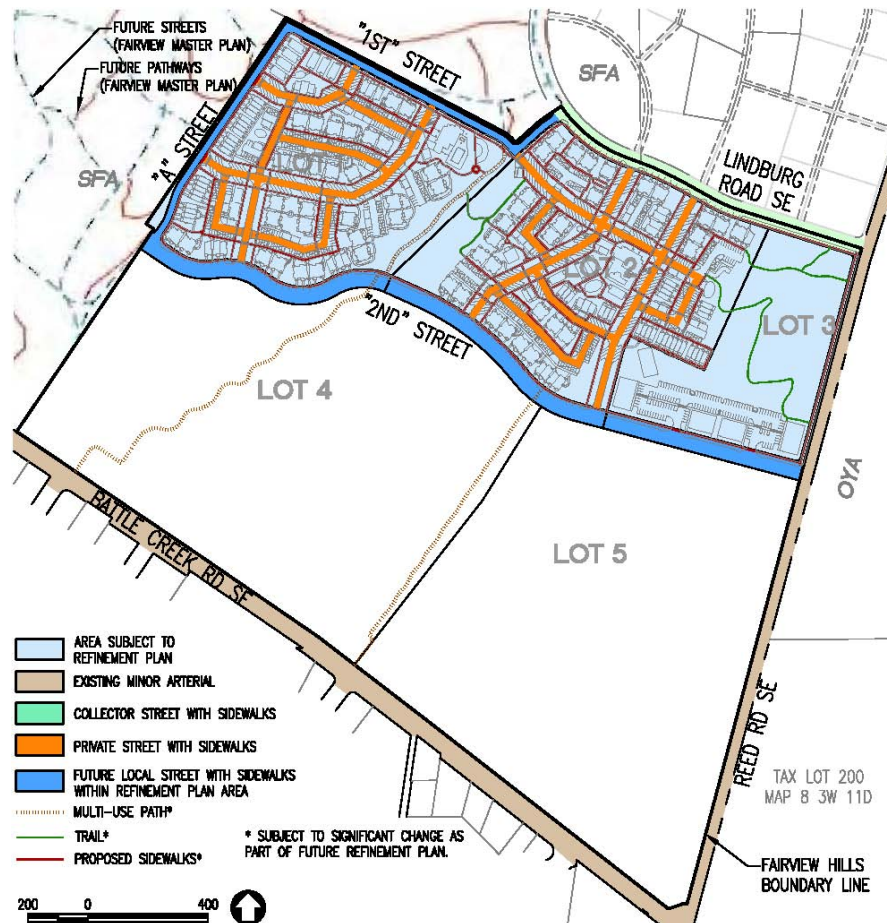


Figure 12: Proposed and Existing Streets & Pedestrian Links Concept

## TYPICAL STREET SECTIONS (4)

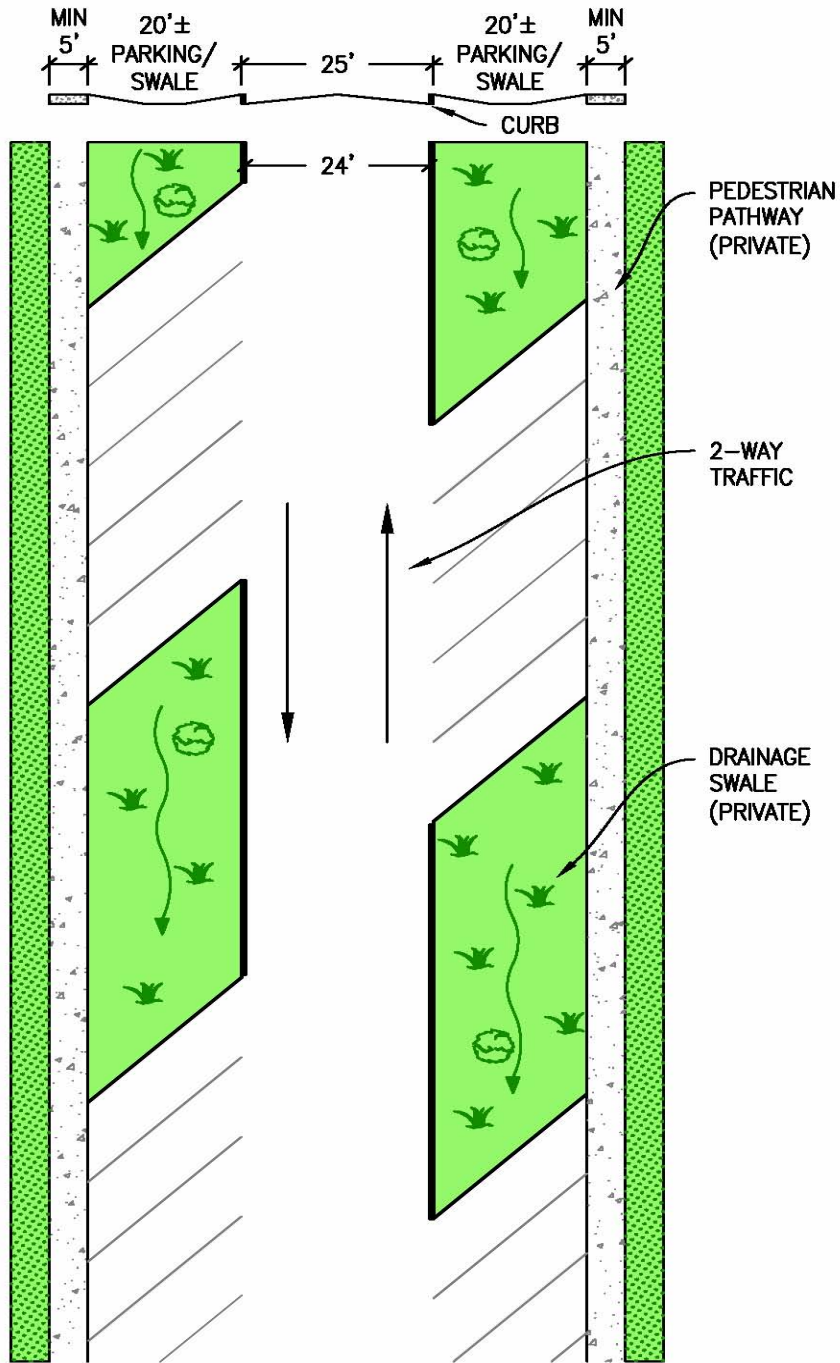
The local public streets within the Refinement Plan area are necessary and will serve many users, including future residents within the site, existing and future residents surrounding the site, service providers (including emergency vehicles, transit providers (including school bus, Salem-Keizer transit), etc. Public ownership and maintenance of streets providing public access and benefits as described above provide the best public safety, provide flexibility for transit shelters/stops, allow for economical future maintenance, ensure a minimum practical design life of 20 years, and handle the typical traffic needs of the City of Salem.

Private streets within the developments will provide the necessary pedestrian, bicycle, and vehicle connections to the bordering public street network. This will allow for greater flexibility in the use of planter strips for storm water facilities.

Public and private streets, multi-use pathways, and trails will generally conform to the typical sections illustrated below. Together, the proposed public and private street system (including sidewalks), trails, and multi-use path system supports a variety of transportation choices for moving people that is convenient and efficient.

As shown on the Illustrative Site Plan and General Landscape Plan curb bulbs are planned at intersections where on-street parking is provided to reduce pedestrian crossing lengths and reduce vehicle speeds.





**Figure 13: Private Street typical conceptual plan view**

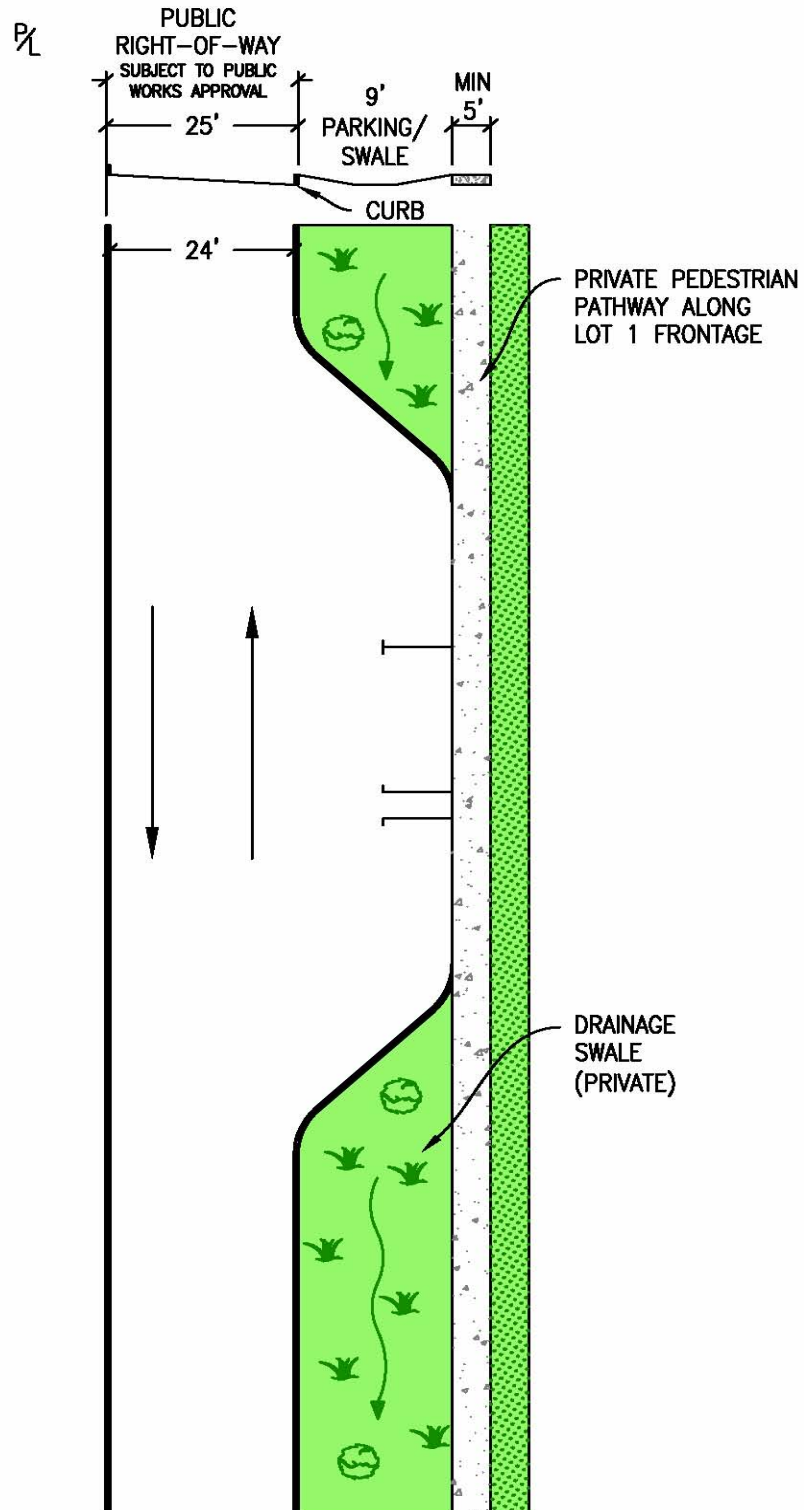
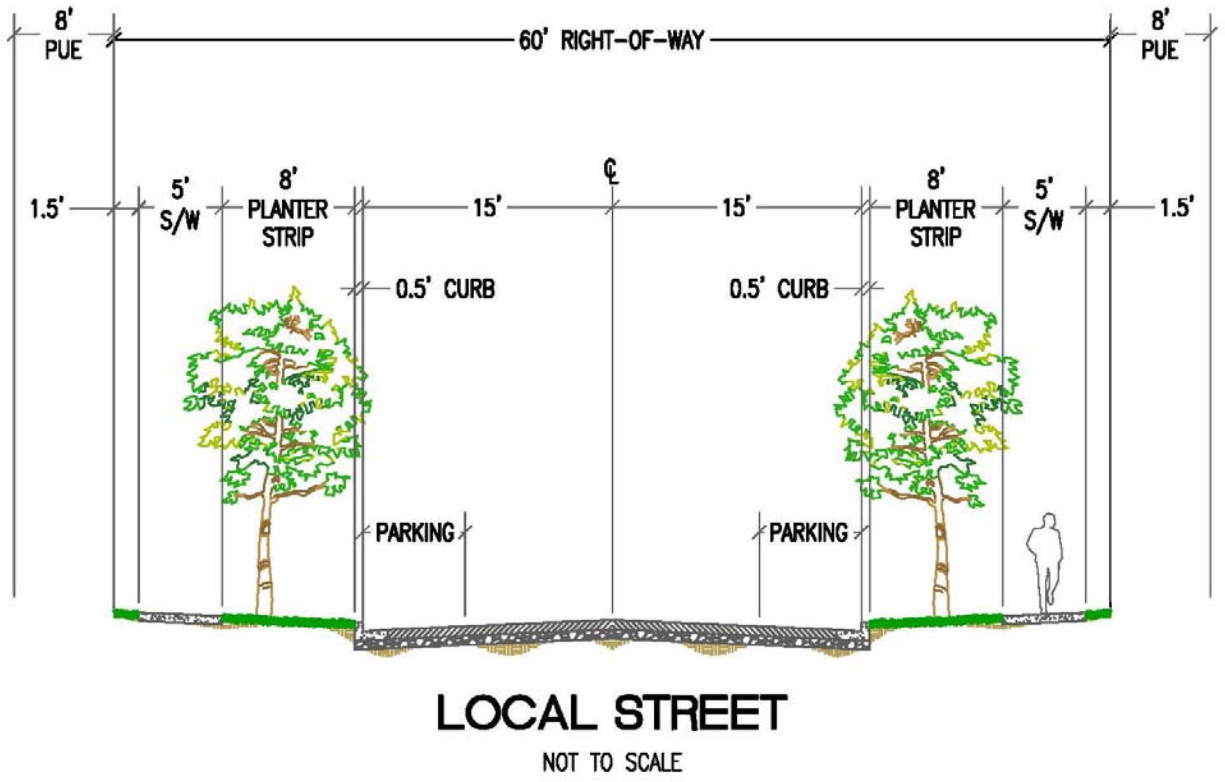


Figure 14: "1st" Street and "A" Street typical public street plan view



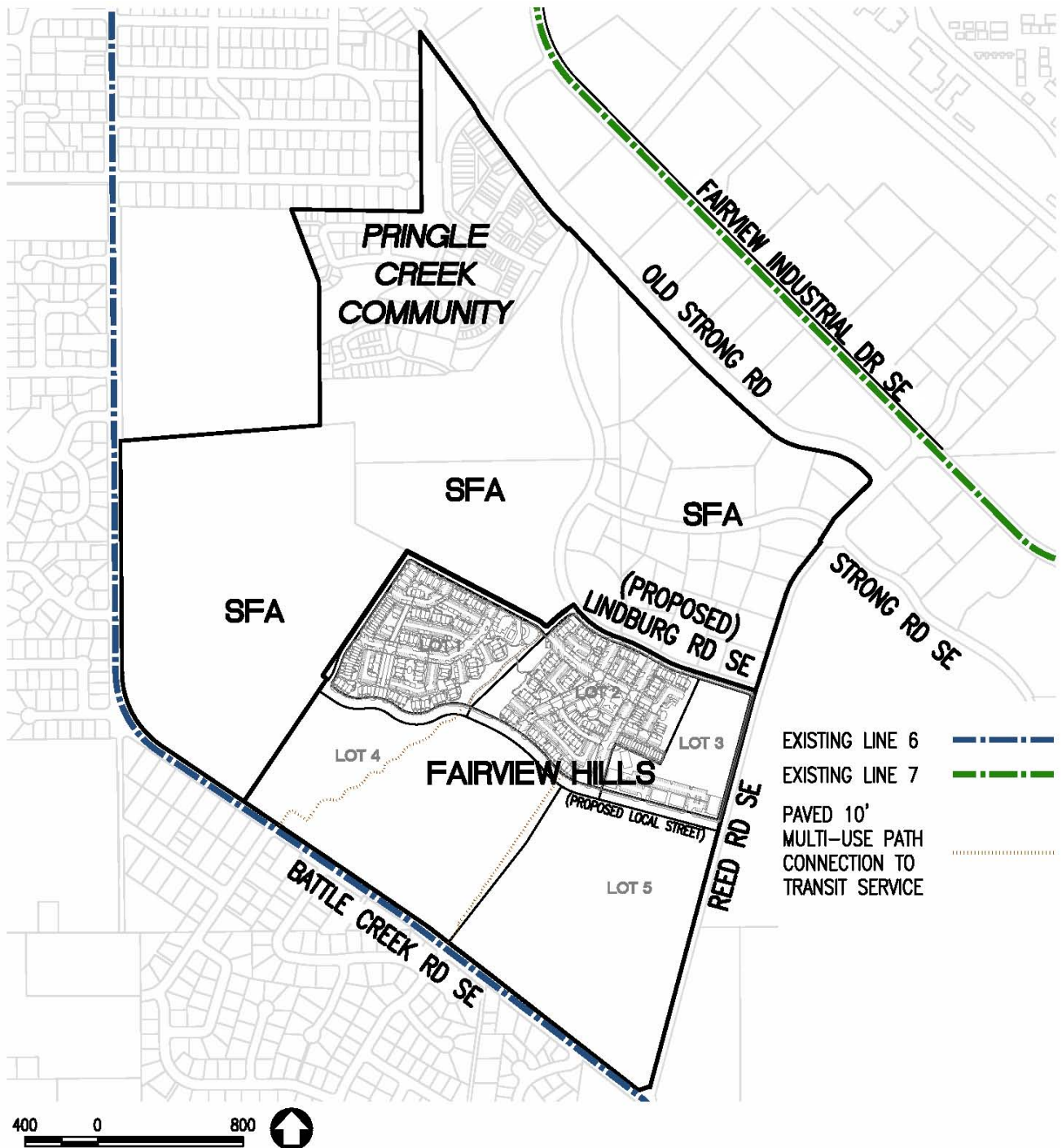
**Figure 15: "2nd" Street typical public street cross section from Reed Road SE to "A" Street**



**Figure 16: Conceptual multi-use path cross section**



**Figure 17: Conceptual trail cross section**



**Figure 18: Current Transit Service**

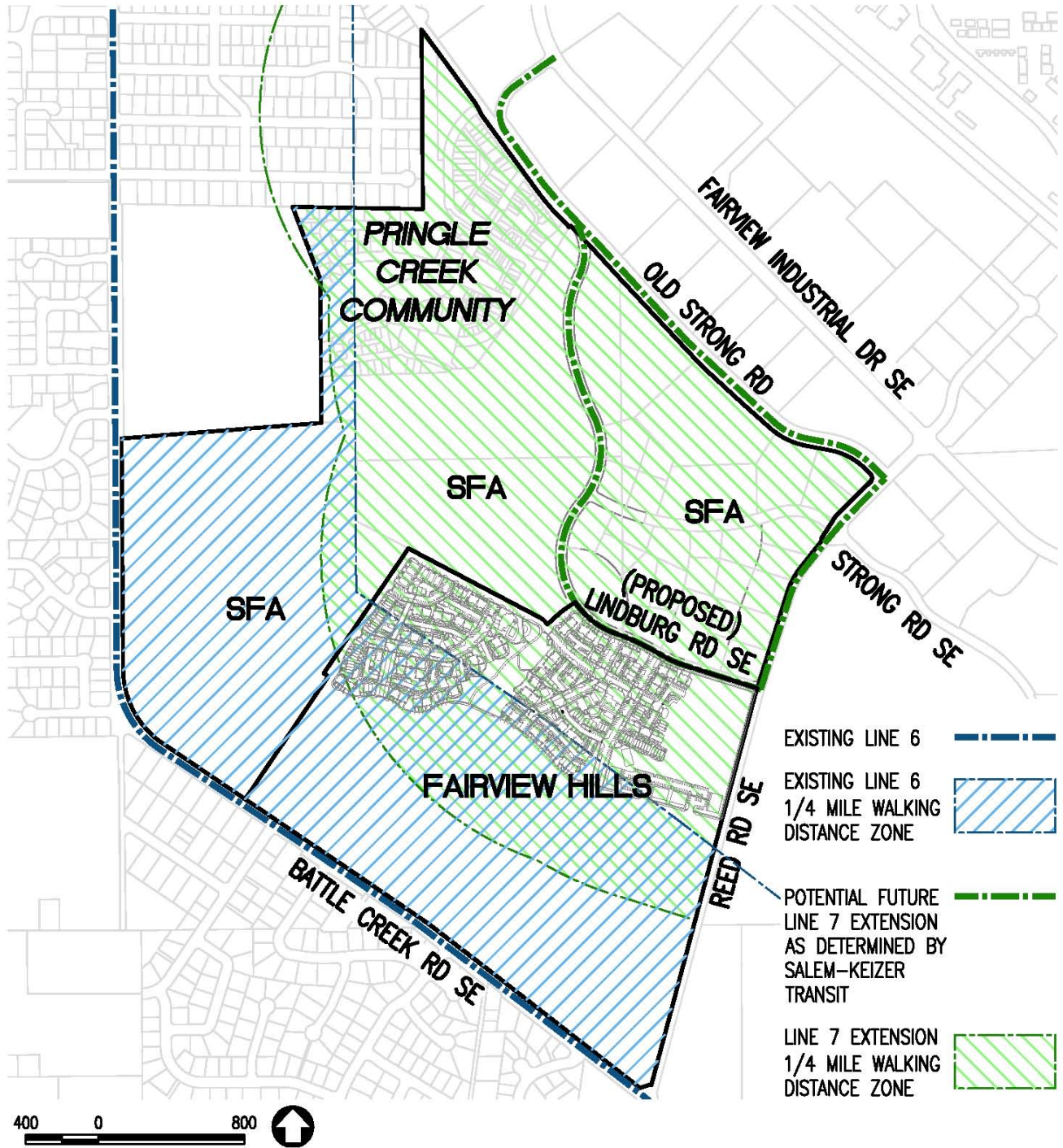


Figure 19: Conceptual Future Transit Plan based on FMP Mobility & Transit Plan

## PERMITTED LAND USES (5)

The overlay designation within the Refinement Plan area is Mixed-Intensity (MI) as designated on the FMU Overlay Plan (Figure 143C-2) and generally described in SRC 143C.040.(b). Permitted uses are as identified in Table 143C-1 of Salem Revised Codes (SRC) Chapter 143C, including refinements in Section (6), as of the date of submittal of this Refinement Plan. The MI overlay permitted uses include all typical residential uses and a variety of neighborhood commercial, employment, and civic uses. Permitted uses of land as well as applicable residential densities are described in Section (2). It is understood that the City of Salem is currently in the process of developing a policy-neutral code re-write changing from SIC to 'use' based classifications. Based upon this understanding, the uses currently permitted in the FMU zone (including those proposed in this refinement plan) will continue to be permitted upon completion of the City Code update. Please also refer to Sections (6) and (15) for additional information regarding permitted uses.

## DEVELOPMENT STANDARDS FOR FMU ZONES (6)

Development standards, regulations and guidelines in this Refinement Plan incorporate the general intent of the adopted Fairview Master Plan and provide for development consistent with the Fairview Mixed Use (FMU) zone per SRC Chapter 143C.

Development will comply with the pertinent sections of the SRC including, but not limited to, Chapters 68, 69, 125, 126, 130, 131, 132, 133, and 143C, as well as the Development Design Handbook (DDH), unless modified herein. Commercial development within the Refinement Plan Area will comply with the development standards of SRC 153 as well as other pertinent sections of the SRC, unless otherwise modified herein. A goal of the Refinement Plan is to meet the existing standards except where deviations will result in an improved project design, increased opportunities for economic development, and/or compliance is otherwise impracticable due to existing conditions such as topographic grade, tree preservation, wetlands, and finish grade transition requirements.

Multi-family development will either meet the standards contained in the City of Salem Development Design Handbook (DDH) including compliance with the refinements below or will be reviewed by the Planning Commission pursuant to the DDH guidelines and SRC Chapter 120. For the purposes of this Refinement Plan, the term apartment shall include the proposed duplex, townhome, and carriage units. Multi-family development is defined below and in DDH Appendix A.

The following is a summary of the refinements to the Salem Revised Codes (SRC) and City of Salem Development Design Handbook (DDH).

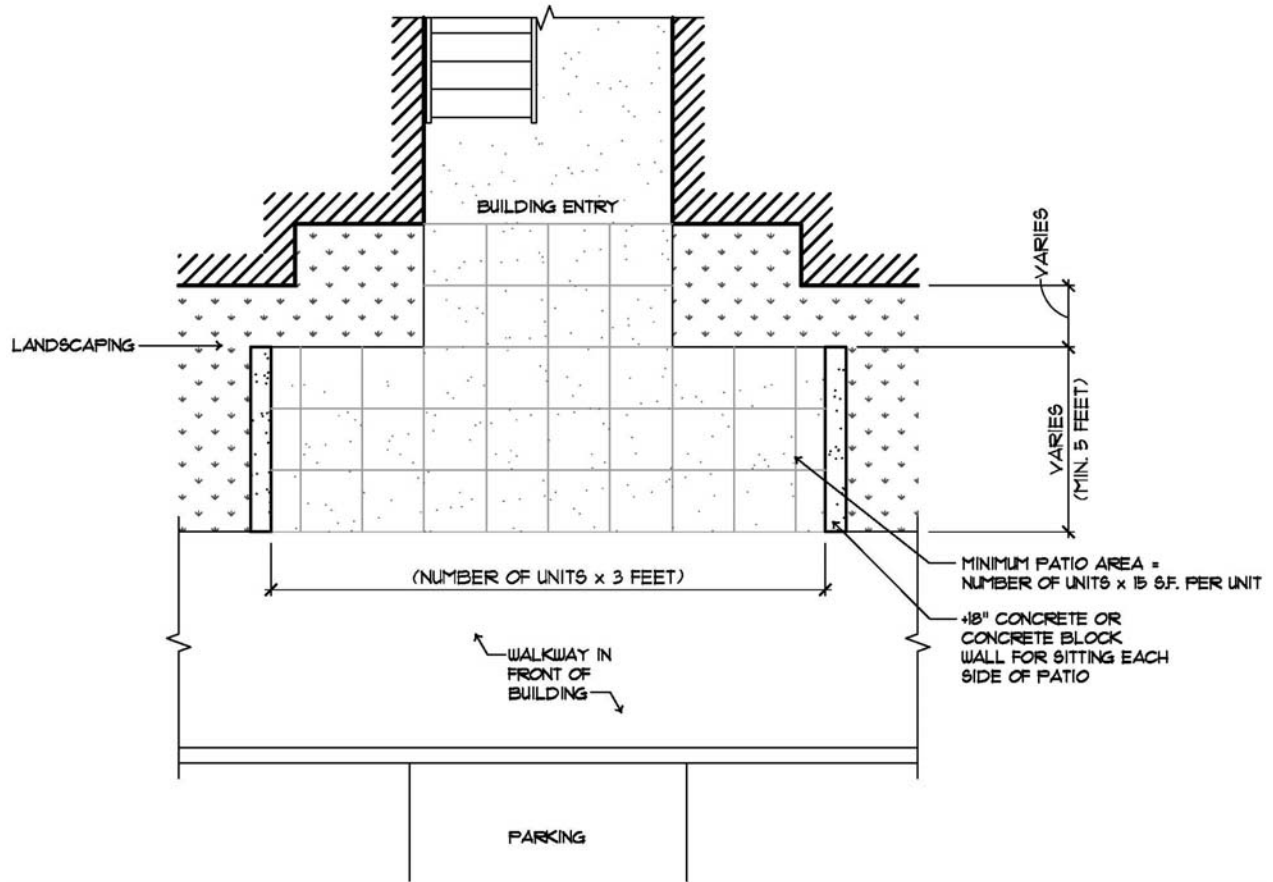


Section	Current Text	Proposed Text
<b>SRC 143C.110.(b)(2)(C)</b>	A maximum 20' setback from all street rights-of-way in all Overlay Areas, except the L1 area, is required for all primary structures.	A maximum 20' setback from all street rights-of-way in all Overlay Areas, except the L1 area, is required for all primary structures. <b>This provision does not apply to buildings separated from a street right-of-way by a wetland, drainageway, stormwater management features, protected trees, and/or slopes exceeding 25 percent.</b>
<b>SRC 143C.110.(b)(6)(A)</b>	All single family attached homes and multi-family residential complexes shall have their primary orientation to the street. Entrances to multi-family buildings may include entrances to individual units or breezeway/courtyard entrances (i.e. to a cluster of residential units); or	All single family attached homes and multi-family residential <b>buildings</b> shall have their primary <b>entry(s)</b> orientation <b>toward</b> the street <b>for buildings adjacent to the public street right-of-way.</b> Entrances to multi-family buildings may include entrances to individual units or breezeway/courtyard entrances (i.e. to a cluster of residential units). <b>Buildings not located adjacent to the public street right-of-way shall have their primary entry(s) orientation towards private drives, courtyards, and/or possible pedestrian lanes where possible; or</b>
<b>SRC 143C.110.(b)(6)(B)</b>	All single family attached homes and multi-family residential complexes may have its primary orientation to a side yard when a direct pedestrian walkway is provided between the main entrance and the street, with at least one entrance located not more than 20' from the curb line of the street.	<b>90% of all townhouse, duplex and multi-family residential buildings shall have their primary orientation either to a public or private street, or to a common or natural open space area. Multi-family residential buildings abutting a street may have their primary orientation to a side yard when a direct pedestrian walkway is provided between the building and the street, with at least one residential entrance located not more than thirty (30) feet from the sidewalk abutting the street. Entrances to multi-family buildings may include entrances to individual units or breezeway/courtyard entrances (i.e. to a cluster of residential units). "Primary Orientation" means the side of the building with the majority of the residential unit entries.</b>

DDH 2.A.4.b.3)	Separate visually and physically ground level private open space from common open space through the use of perimeter landscaping or fencing	<i>(Delete this provision as it not consistent with the Fairview Master Plan.)</i>
DDH 2.A.4.b.4)	Design and construct carports, garages and/or parking areas that are not located within twenty (20) feet of public right-of-way.	Design and construct carports, garages and/or parking areas that are not located within twenty (20) feet of public right-of-way. <b>This provision does not apply to public or private on-street parking.</b>
DDH 2.D.2.b.3)	Separate pathways the connect buildings, open spaces, and parking areas from the dwelling by a minimum distance of ten (10) feet. The separation is measured from the pathway edge closest to any dwelling unit.	Separate pathways that connect buildings, open spaces, and parking area from the dwelling by a minimum distance of <b>five (5) feet</b> . The separation is measured from the pathway edge closest to any dwelling unit <b>wall</b> .
DDH 2.D.3.b.1)	Provide pedestrian connectivity from the site to the public sidewalk system through the use of paths or easements.	<b>Interior pedestrian pathways width will be 5 feet wide unless existing site conditions warrant exceptions for minor segments of pathways.</b>
DDH 2.D.3.b.5)	Install a wall, fence, or landscaping to buffer parking areas from public right-of-way or abutting properties.	<b>An exemption to this standard is permitted for parking along private streets.</b>
DDH 2.E.2.b.2)	Design and construct buildings that have no dimension greater than one-hundred and fifty (150) feet	<b>Exterior finishes materials are subject to the following standards:</b>  <b>1. Plain concrete block, plain concrete, corrugated metal, plywood and sheet pressboard are not allowed as exterior finish material, except as secondary finishes if they cover no more than 25% percent of the surface area of each facade. Composite boards manufactured from wood or other products, such as hardboard or hard plank, may also be used.</b>  <b>2. Where wood products are used for siding, the siding must be shingles, horizontal siding or board and batten.</b>

		<p>3. Where horizontal siding is used, it must be shiplap or clapboard siding, or vinyl or aluminum siding which is in a clapboard or shiplap pattern.</p> <p>4. Trim must mark all building roof lines, porches, windows and doors on all elevations. The trim must be at least four (4) inches nominal wide. Buildings with an exterior material of stucco or masonry are exempt from this standard.</p>
DDH 2.E.3.b.3)	On sites with 75' or more of buildable width, occupy at least 50 percent of the buildable width by a building placed on the setback line.	On sites with 75' or more of buildable width, occupy at least 50 percent of the buildable width by a building placed on the setback line. <b>This provision does not apply to buildings separated from a street right-of-way by a wetland, drainageway, stormwater management features, protected trees, and/or slopes exceeding 25 percent.</b>
DDH 2.E.3.b.5)	<p>Incorporate into buildings a porch or architectural defined entry space for each ground floor level dwelling unit. Shared porches or entry spaces are permitted provided that the porch or entry area is at least 25 square feet in area per dwelling unit, with no dimension less than 5 feet for each unit. Porches and entry areas shall be open on at least one exterior side, and may be covered or uncovered. All grade level porches shall include hand railings, half walls, or shrubs to define their outside perimeter.</p>	<p>Incorporate into buildings a porch or architectural defined entry space for each ground floor level dwelling unit. Shared porches or entry spaces are permitted provided that the porch or entry area is at least 25 square feet in area per dwelling unit, with no dimension less than 5 feet for each unit <b>for two to four units; and at least 15 square feet in area per dwelling unit, with no dimension less than 3 feet for each unit for five or more units. The overall space shall be square, rectangular, and/or otherwise usable with one of the space dimensions meeting the minimum dimensional requirement. In no case shall the minimum multiple unit space dimension be less than 5 feet (see Figure 20 for an illustrative diagram).</b> Porches, entry and unit access stair landing areas shall be open on at least one exterior side, and may be covered or uncovered. All grade level porches shall include hand railings, half walls, or shrubs to define their outside perimeter.</p>

DDH 2.E.4.b.1)	Offset every two (2) attached dwelling units from the next dwelling unit by at least four (4) feet in depth (See graphics below with the numbers identifying examples of what are considered offsets.)	<b>Offset stacks of units as viewed in plan from the adjacent dwelling unit(s) from the adjacent dwelling unit(s) by at least two (2) feet in depth horizontally. Offset stacked dwelling units from each other to differentiate the building base and top from the other floor(s). Offsets must be at least two (2) feet over at least 25 percent of the unit façade. Intervening roofs shall count as offsets. (See Figure 21A and 21B below identifying some of the examples of what are considered offsets.) Section does not apply to duplex structures.</b>
DDH 2.E.4.b.2)	Within twenty-eight (28) feet from any property line, the building setback for adjacent buildings on the same lot shall vary by at least four (4) feet.	Within twenty-eight (28) feet from any property line, the building setback for adjacent buildings on the same lot shall vary by at least <b>two (2)</b> feet.
DDH 2.E.4.b.3)	When providing a common entrance, limit the access to not more than four (4) dwelling units.	When providing a common entrance, limit the access to not more than six (6) dwelling units.
DDH Section10 – Appendices, Appendix A Definition of Terms- “Multiple Family Development”	Any building, or portion thereof, which is designed, built, rented, leased, let or hired out to be occupied, or which is occupied as the home or residence of three or more families living independently of each other and doing their own cooking in the said building; or a building in condominium ownership containing three or more dwelling units.	Any building, or portion thereof, which is designed, built, rented, leased, let or hired out to be occupied, or which is occupied as the home or residence of three or more families living independently of each other and doing their own cooking in the said building; or a building in condominium ownership containing three or more dwelling units. <b>Town Home, Duplex (two dwelling units in one building) and Carriage House (dwelling unit(s) located above a parking garage) may be included within a “Multiple Family Development” if total number of Townhome, Duplex and Carriage House dwelling units does not exceed 40% of the total dwelling units in the development and the development standards for the Townhome, Duplex and Carriage House buildings are the same as multi-family buildings unless exempted by individual code sections.</b>



**Figure 20: Illustrative Diagram for DDH 2.E.3.b.5) as indicated in the above table**



**Figure 21A: Example Building Front Offset for refinement to DDH 2.E.4.b.1) as indicated in the above table**



**Figure 21B: Example Building Back Offset for refinement to DDH 2.E.4.b.1) as indicated in the above table**

## **STANDARDS FOR CONSERVATION OF NATURAL RESOURCES (7)**

Development within the Refinement Plan area will conform to the following portions of the SRC specifically adopted to address conservation of natural resources within the City.

- Chapter 68 – Preservation of Trees and Vegetation
- Chapter 69 – Landslide Hazards
- Chapter 126 – Wetlands

In addition to complying with these regulations and standards, the development will incorporate the following principles.

### **Cultural Resources**

In the unlikely event that cultural resources are encountered during construction on the site, the Oregon State Historic Preservation Office (SHPO) should immediately be notified and work halted in the vicinity of the finds until they can be inspected and assessed. An inadvertent discovery plan has been included in the Appendix.

There are several potential cultural sites identified in the Archaeological Cultural Resources Inventory & Assessment of the Fairview Master Plan dated August 2004 (Exhibit 6), as well as the Appendix A dated June 2004, as shown on Figure 23 below. This map has been included at the request of City staff. It should be noted that in 2009 and 2010, extensive cultural resource investigations were conducted by Applied Archaeological Research, Inc. (AAR) of the entire 103.8± acre Fairview Hills (Simpson Hills, LLC) property suggesting “*the development contains very sparse archaeological remains.*” A copy of the AAR reports has been included in the Appendix. See also Section 24 of this Refinement Plan document for further information.

Documentation was previously provided to SHPO to document compliance with SHPO requirements as part of the demolition of three buildings on, or partially on the property. This documentation is included in the Appendix of this Refinement Plan.

### **Trees**

The grove of Oregon White Oaks located in the northeast corner of the refinement area adjacent to Lindburg Road SE and Reed Road SE is to be preserved as a gateway to the development in conformance with the adopted Fairview Master Plan.

### **Surface Water**

Two existing uphill drainage basins (shown on Figure 35) discharge to the Fairview Hills property through existing culverts crossing underneath Battle Creek Road SE. Infiltration testing has been performed by GeoPacific Engineering, Inc. in several areas throughout the Fairview Hills property. The estimated long term sustainable vertical infiltration rates at the site are on the order of 0.1 to 0.5 inches per hour or less making the degree of infiltration that was envisioned in the Fairview Master Plan impractical. The GeoPacific Geotechnical Investigation report along with several additional geotechnical letters has been included in the Appendix.

To address the specific drainage characteristics that exist on-site, a multi-faceted approach to stormwater management is proposed. This includes layered green infrastructure techniques that are tailored to individual portions of the site. This layered approach includes street-side bioswales, vegetated filter strips within wide green open space corridors, infiltration planters, and areas of permeable pavement. To accommodate the large volumes of on and off site drainage, an extended dry detention basin is also proposed. This provides both water quantity attenuation and water quality treatment that is appropriate for this setting given the existing soils, hydrology, topography, and surrounding environment. It will be designed in accordance with the Low Impact Development Approaches Handbook adopted by Clean Water Services of Washington County.

The facility will be integrated into the surrounding landscaping and will appear as a shallow landscaped depression with a flat bottom that collects and holds stormwater runoff, allowing pollutants to settle and filter out as the water infiltrates into the ground (to the extent possible) prior to release. The entire facility area, including side slopes and treatment area will feature vegetation that is appropriate for the varying conditions within the extended dry basin. Vegetation will be established through dense plantings which will include a variety of native plants as well as non-invasive ornamentals that provide aesthetic and functional value. Species will be selected to assure successful establishment based on climate, soil type, and moisture tolerance. Selections will be made to ensure that they are harmonious with the surrounding sensitive areas and existing vegetation.

### **Soils**

Minimization of potential erosion of the on-site soils is critical to protecting local streams, wetlands, and drainage courses. Development will comply with City of Salem erosion control standards. Construction areas disturbing more than one (1) acre shall obtain a National Pollutant Discharge Elimination System (NPDES) 1200C permit from the Oregon Department of Environmental Quality (DEQ). Proof of a valid DEQ permit must be submitted to the City of Salem prior to issuance of building permits. GeoPacific Engineering, Inc. has completed a Geological Hazard Assessment Report for the site which has been included in the Appendix.

### **Reuse of on-site Materials**

Construction will reuse the substantial amount of gravel and crushed rock piles that currently exist from prior work on the property when feasible. Utilizing this material for road bases, trenches and structural backfill will reduce the need for importing these materials thereby reducing the impact of construction traffic on local roads and infrastructure. The reuse of materials will also help reduce the carbon footprint.

### **Vegetation**

Vegetative cover is crucial to the health of the soil structure and the animals that traditionally inhabit the South Salem hills. Establishment and re-establishment of vegetation native to the area is important to the successful maintenance of natural areas. Re-establishment of vegetation within the limits of construction will also be critical to minimizing the potential for erosion and shall be incorporated into construction drawings for development within the Refinement Plan area.

### **Wildlife**

The incorporation of open space and re-establishment of vegetation native to the Willamette Valley will provide habitat inviting local wildlife to share in the diversity of the site that is not typically present in other development projects.



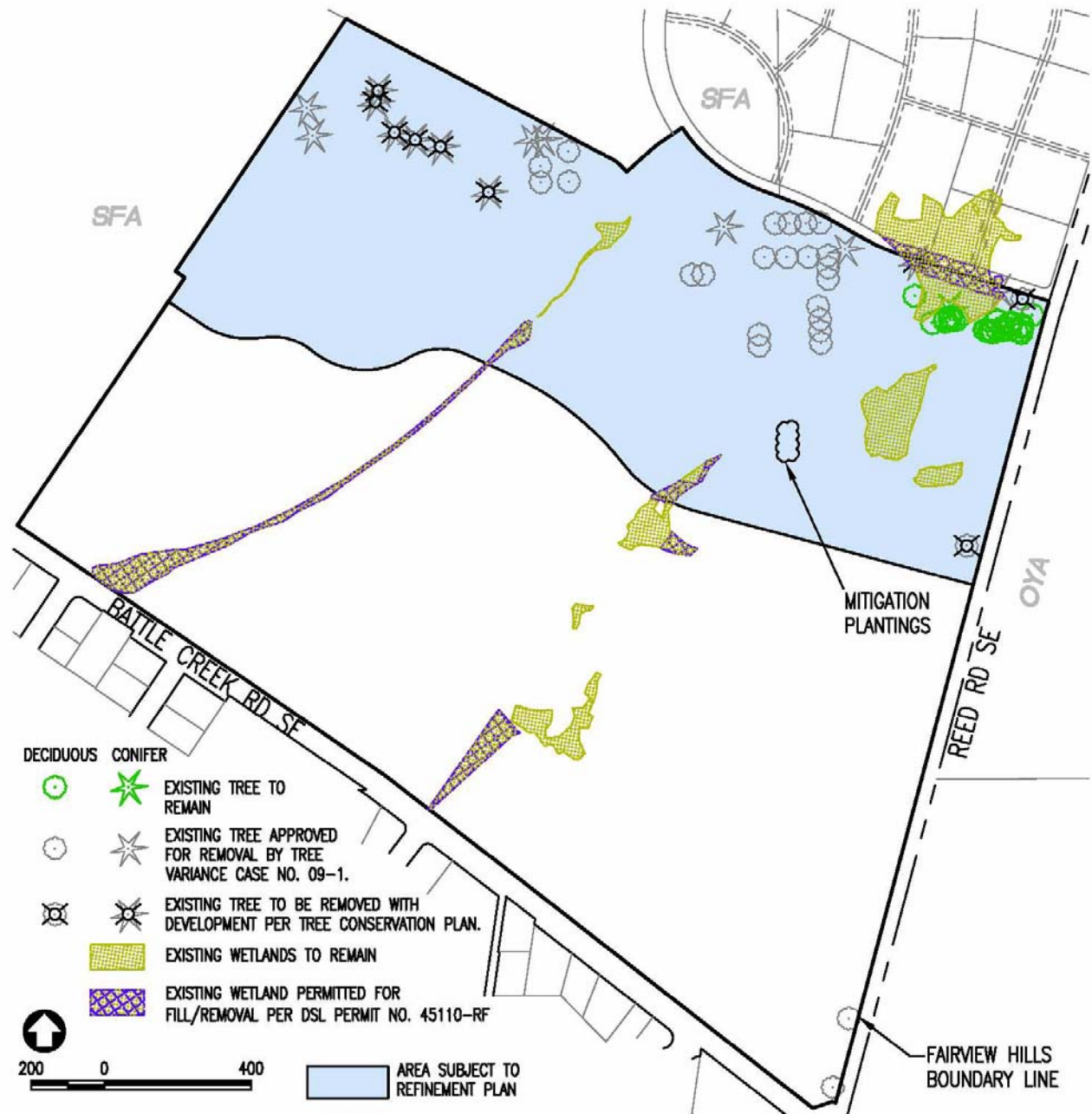
## WETLAND AND TREE INVENTORY (8)

### Wetlands

The wetland map included in the Natural Resources Inventory of the Fairview Master Plan (Exhibit 4 – Figure 5) is outdated. The wetland map prepared by W&H Pacific in 2003 and approved by the Department of State Lands (DSL) on July 15, 2004 expired July 15, 2009. PBS Engineering + Environmental were retained to complete a wetland delineation of the Fairview Hills property in 2009. The PBS wetland delineation was submitted to DSL and the U.S. Army Corps of Engineers (USACE) on September 1, 2009 which was approved by DSL on June 1, 2010 and the USACE on July 6, 2010. The DSL issued a removal/fill permit for the site which identifies the areas of permanent and temporary impacts and outlines the mitigation requirements for the entire Fairview Hills property. Several wetlands will remain on the property and be incorporated into the site's open space. The wetlands regulated by the current permit are shown on Figures 22 and 23.

### Tree Inventory

A certified professional arborist evaluated the trees on the Fairview Hills property. Trees were identified by species, type, size and general condition. The Refinement Plan area has several existing trees that will be retained in accordance with the City approved May 4, 2009 tree regulation variance (case no. 09-1). Preservation of trees and vegetation within the refinement area will comply with SRC Chapter 68.



**Figure 22: Tree Inventory and Conceptual Preservation Plan**

## **METHODS OF PROTECTION FOR NATURAL FEATURES (9)**

### **Historic Structures**

The site contains no designated historic structures. The State of Oregon Archaeologist issued a letter dated July 7, 2009, a copy of which is included in the Appendix, stating "...the project will have no effect on any known cultural resources. No further archaeological research is needed with this project." In order to further protect any potential discovery of cultural resources and to comply with state law an inadvertent discovery plan is included in the Appendix and shall be included as part of the construction documents for every development on the property.

### **Natural Features**

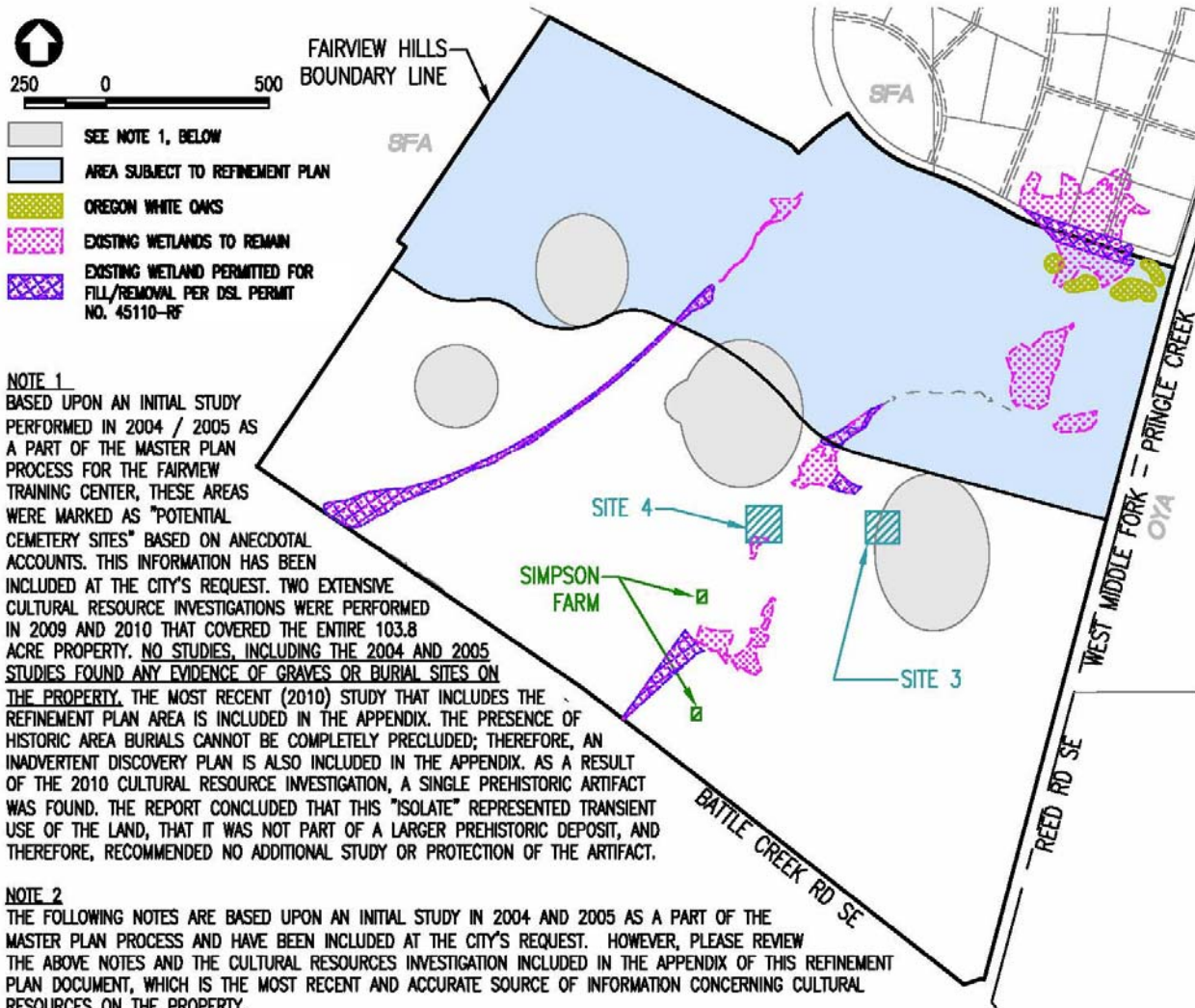
Mature trees existing at the property and existing wetland areas will be protected for incorporation into open spaces where possible. A tree inventory for the property was previously approved by the City of Salem as Tree Variance Case No. 09-1. Any modifications to the previously approved tree conservation plan will be processed as required by SRC Chapter 68.

### **Trees**

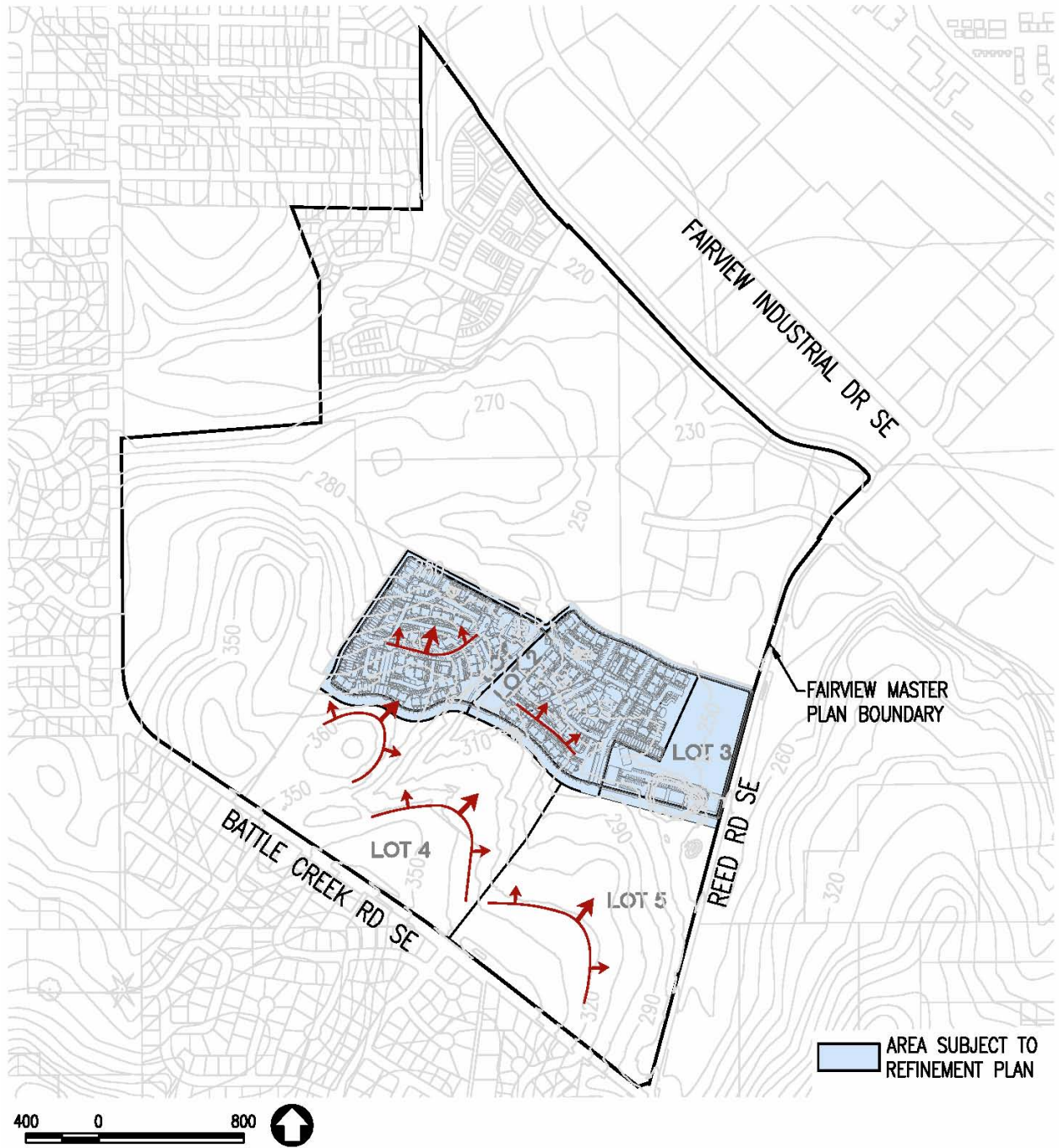
During all clearing, grading and construction activities, protective fencing shall be installed and maintained around all trees designated for retention by the approved tree conservation plan for the property including any adjustments thereto. Any heritage or significant tree shall have protective fencing at least seventy percent of a circular area beneath the tree measuring one foot in radius for every one-inch of diameter at breast height (dbh), or as otherwise allowed by the project arborist.

### **View Sheds**

The view sheds identified in the adopted Fairview Master Plan on the western portion of the Refinement Plan area have natural topography that may provide for views to the north and east. The layout and development of this area of the Refinement Plan incorporates the natural topography into the site design to help maintain and/or enhance views to the north and east where feasible. Figure 24 shows the Refinement Plan area view sheds in the Fairview Master Plan context.



**Figure 23: Historic Features Shown Per 2004 Fairview Plan (See Notes Above)**



**Figure 24: Refinement Plan Areas View Sheds in the Fairview Master Plan Context**

## **MAINTENANCE OF INFRASTRUCTURE (10)**

Public streets and utility infrastructure will be constructed to City standards, unless modified herein, and dedicated to the City of Salem. The City will be responsible for maintaining the public right-of-way areas including streets, sidewalks, lights, trees and public utility infrastructure within the refinement area. The public utility infrastructure typically includes, but is not limited to, public water mains, fire hydrants, valves, manholes, sanitary sewer mains, cleanouts, storm drain mains, manholes & catch basins, public storm water facility, and public street furnishings/trash/recycling cans/etc.

A Property Owner Association (POA) will be created to own and manage privately owned streets, green corridors (drainages), pedestrian/bike paths, private site furnishings/trash cans/recycling, and open space within the refinement area. The POA will establish operation and maintenance standards for the private infrastructure, perpetual maintenance of any open space, and community amenities.

## **CONSTRUCTION PHASING OF STREETS (11)**

Street construction phasing will be tied to building permits for development on each lot within the refinement area. Lots 1, 2, and 3 are located within the Refinement Plan area. Required street improvements common to all phases and lots within the Refinement Plan area are as follows.

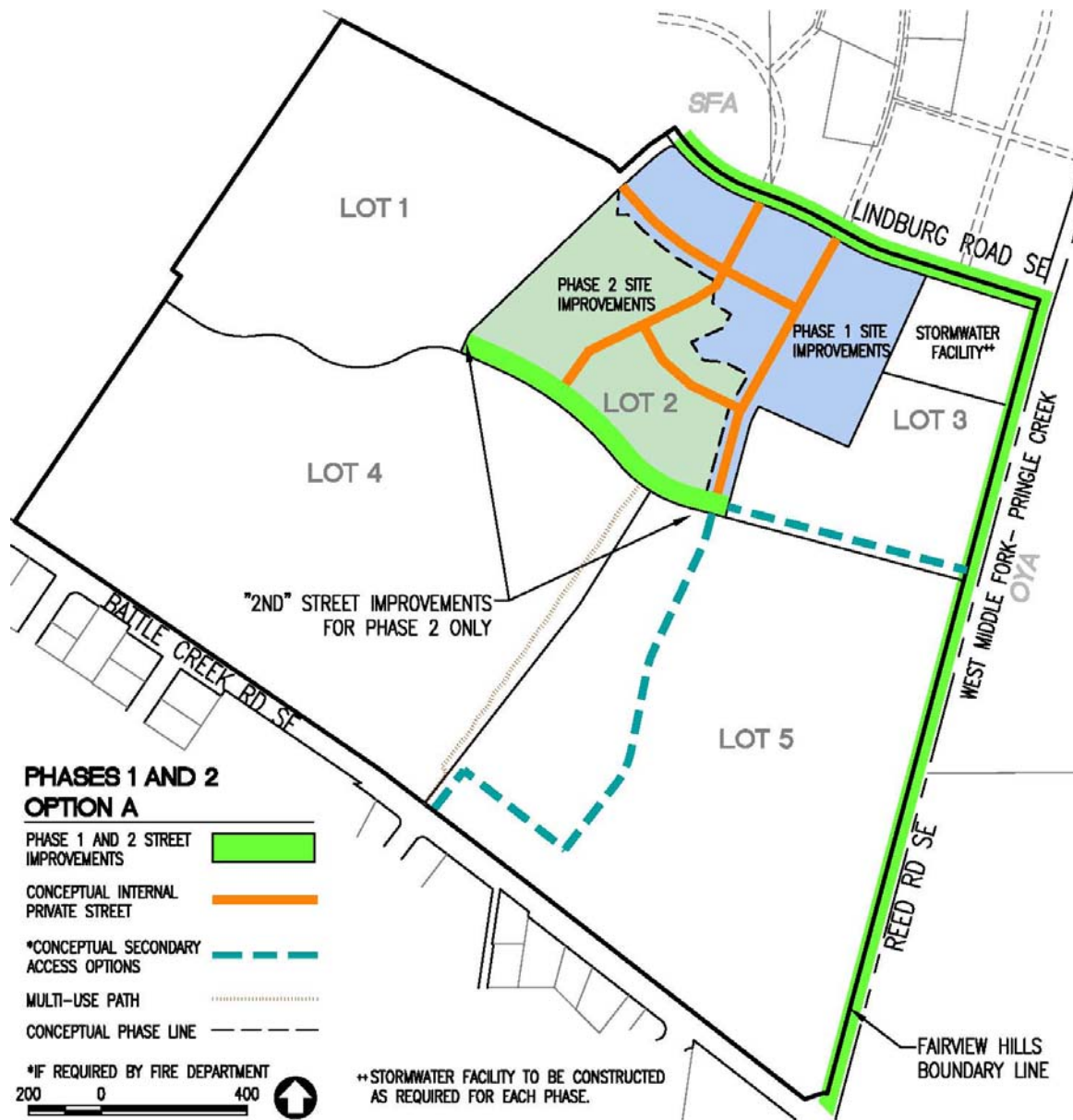
- Reed Road SE street improvement (34 ft. wide linking street) from “2nd” Street to Battle Creek Road SE.
- If secondary access is required by the Salem Fire Department, then a minimum 20 ft. wide secondary access road with all-weather surface shall be constructed from the Refinement Plan area (Lots 1, 2 or 3) to Reed Road SE or Battle Creek Road SE.

In addition to the above required street improvements, there are two options available for the Refinement Plan area. The construction phasing of the streets for both options is summarized as follows.

**Lindburg Road SE construction in cooperation with abutting property owner (Option A)**

**Phases 1 & 2 (Lot 2)**

- Full street improvement of Lindburg Road SE from the west property line of Lot 2 to Reed Road SE.
- Reed Road SE street improvement (34 ft. wide linking street) from Lindburg Road SE to “2nd” Street.
- Construction of the multi-use path from lot 2 to Battle Creek Road SE.
- Phase 2 only - 3/4 street improvements to “2nd” Street for lot frontage.



**Figure 25: Street Phasing for Lot 2 (Phases 1 and 2 – Option A)**

### Phase 3 (Lot 1)

- Full street improvement of Lindburg Road SE from the west property line of lot 1 frontage on Lindburg Road SE to Reed Road SE.
- Reed Road SE improvement (34 ft. wide linking street) from Lindburg Road SE to "2nd" Street.
- Full street improvements along lot frontage for "1st" Street (sidewalk on south side only) and "A" Street (sidewalk on east side only).
- Construction of the multi-use path from the lot to Battle Creek Road SE.

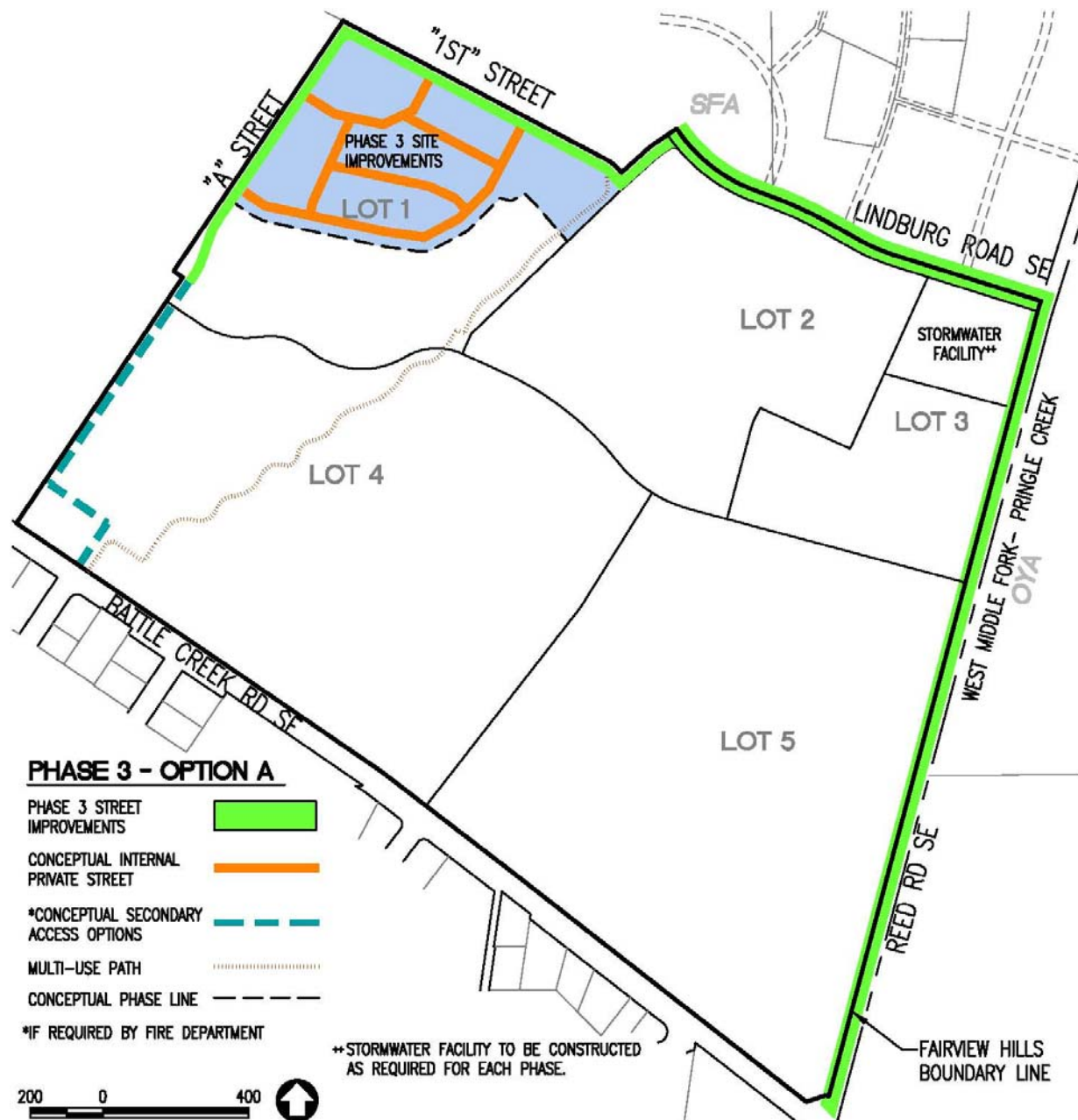
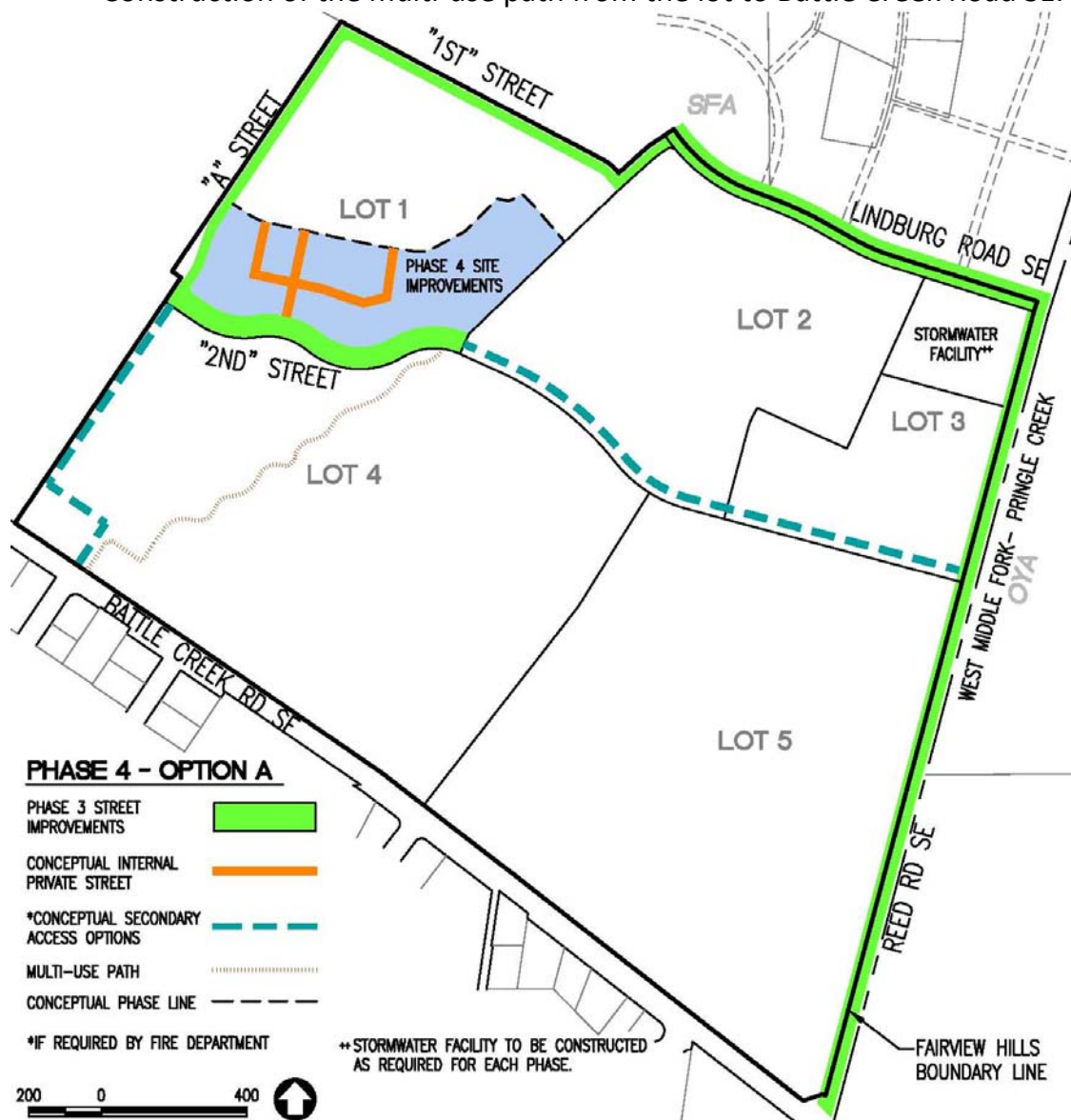


Figure 26: Street Phasing for Lot 1 (Phase 3 – Option A)

## Phase 4 (Lot 1)

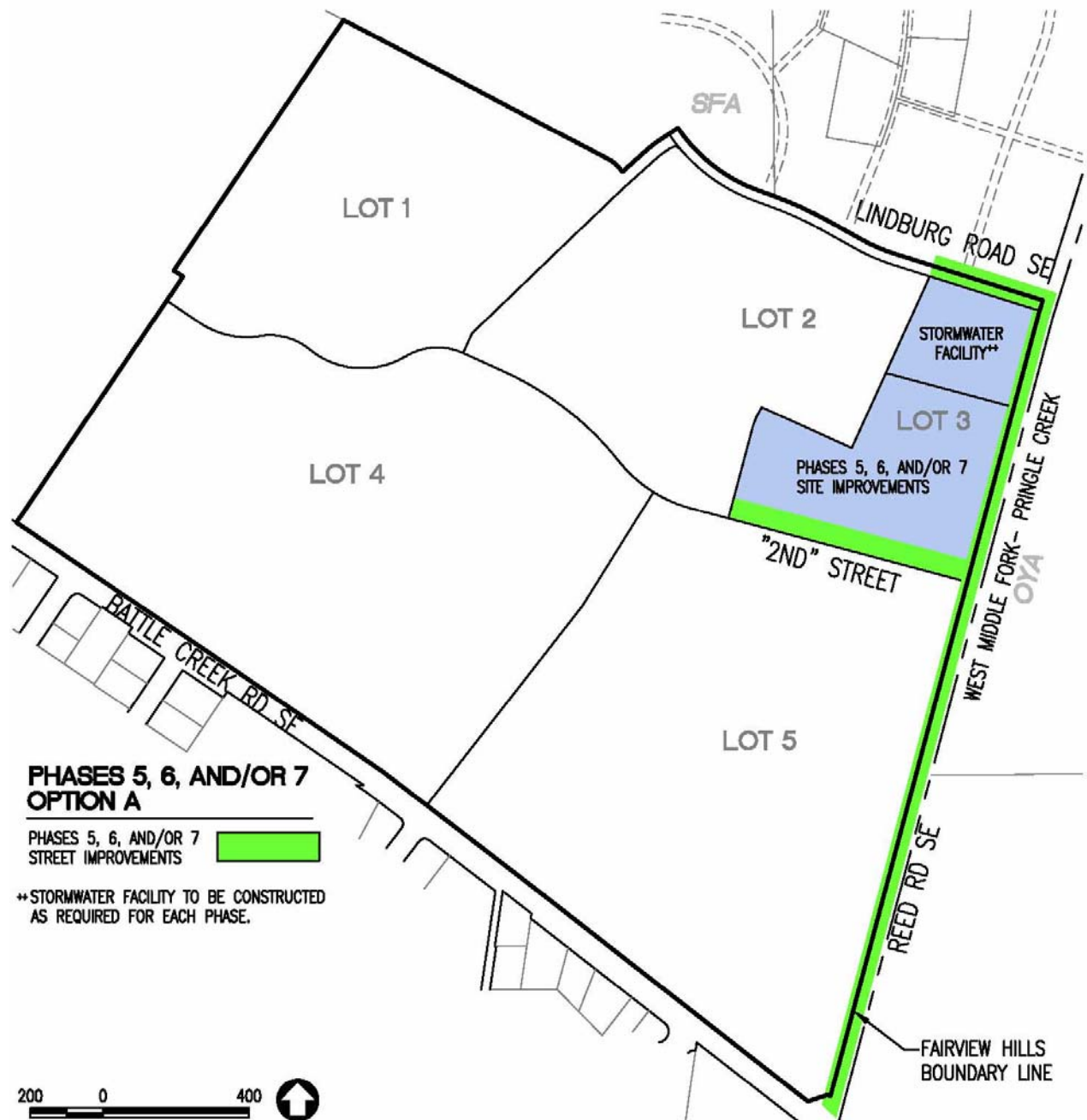
- Full street improvement of Lindburg Road SE from the west property lot line to Reed Road SE.
- Reed Road SE improvement (34 ft. wide linking street) from Lindburg Road SE to "2nd" Street.
- Full street improvement of "1st" Street with sidewalk on south side only from Lindburg Road SE to "A" Street.
- Full street improvement of "A" Street with sidewalk on east side only from "1st" Street to "2nd" Street.
- Phase 4 only – 3/4 street improvements to "2nd" Street for lot frontage.
- Construction of the multi-use path from the lot to Battle Creek Road SE.



**Figure 27: Street Phasing for Lot 1 (Phase 4 – Option A)**

Phases 5, 6, and/or 7 (Lot 3)

- Full street improvement of Lindburg Road SE in cooperation with abutting property owner along the lot frontage from the west property line to Reed Road SE.
- Reed Road SE 3/4 street improvement from Lindburg Road SE to "2nd" Street.
- "2nd" Street 3/4 street improvement along the lot frontage.

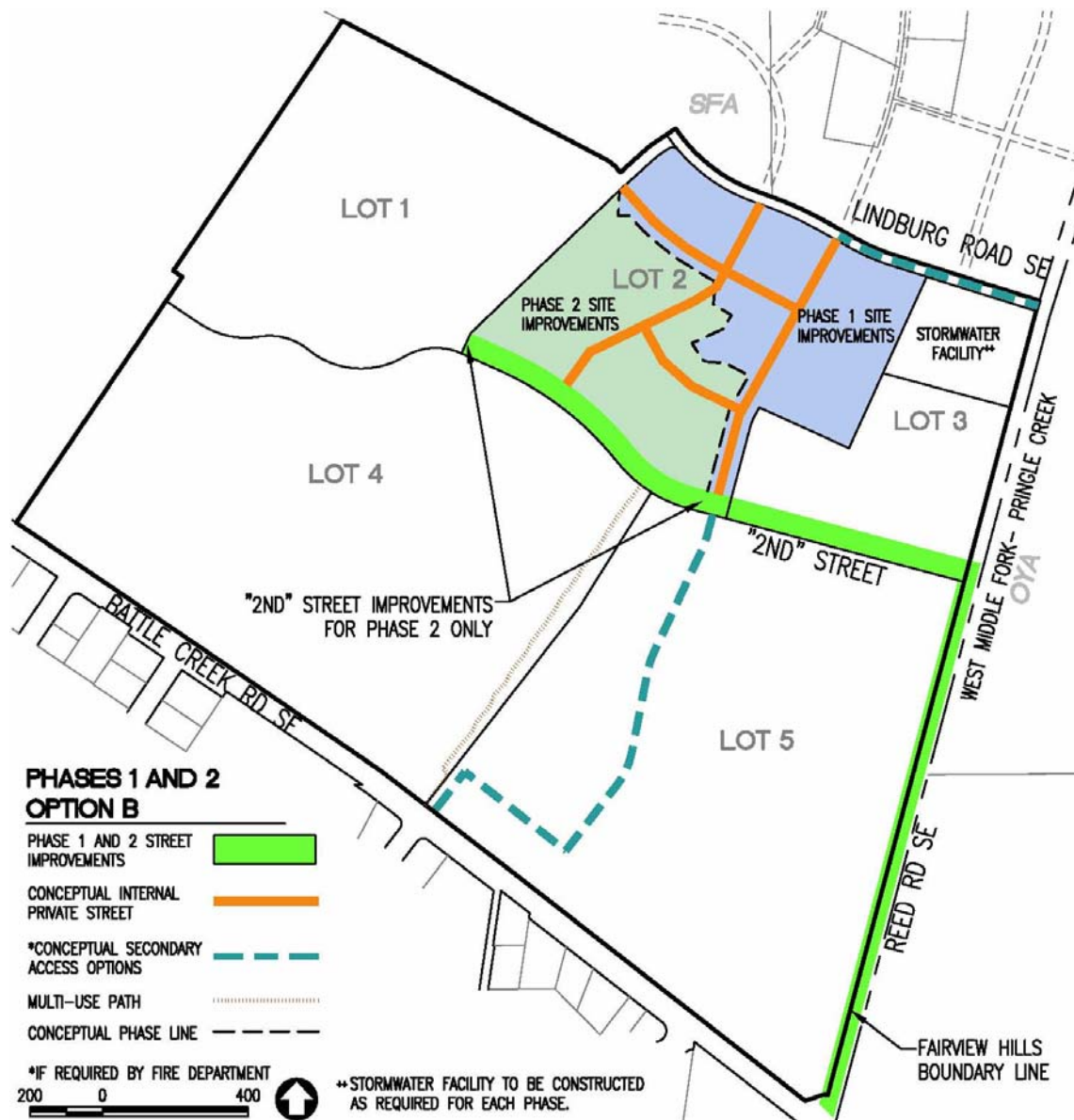


**Figure 28: Street Phasing for Lot 3 (Phases 5, 6 &/or 7 – Option A)**

**Deferral or a fee-in-lieu payment for the lot frontage on Lindburg Road SE (Option B)**

**Phases 1 & 2 (Lot 2)**

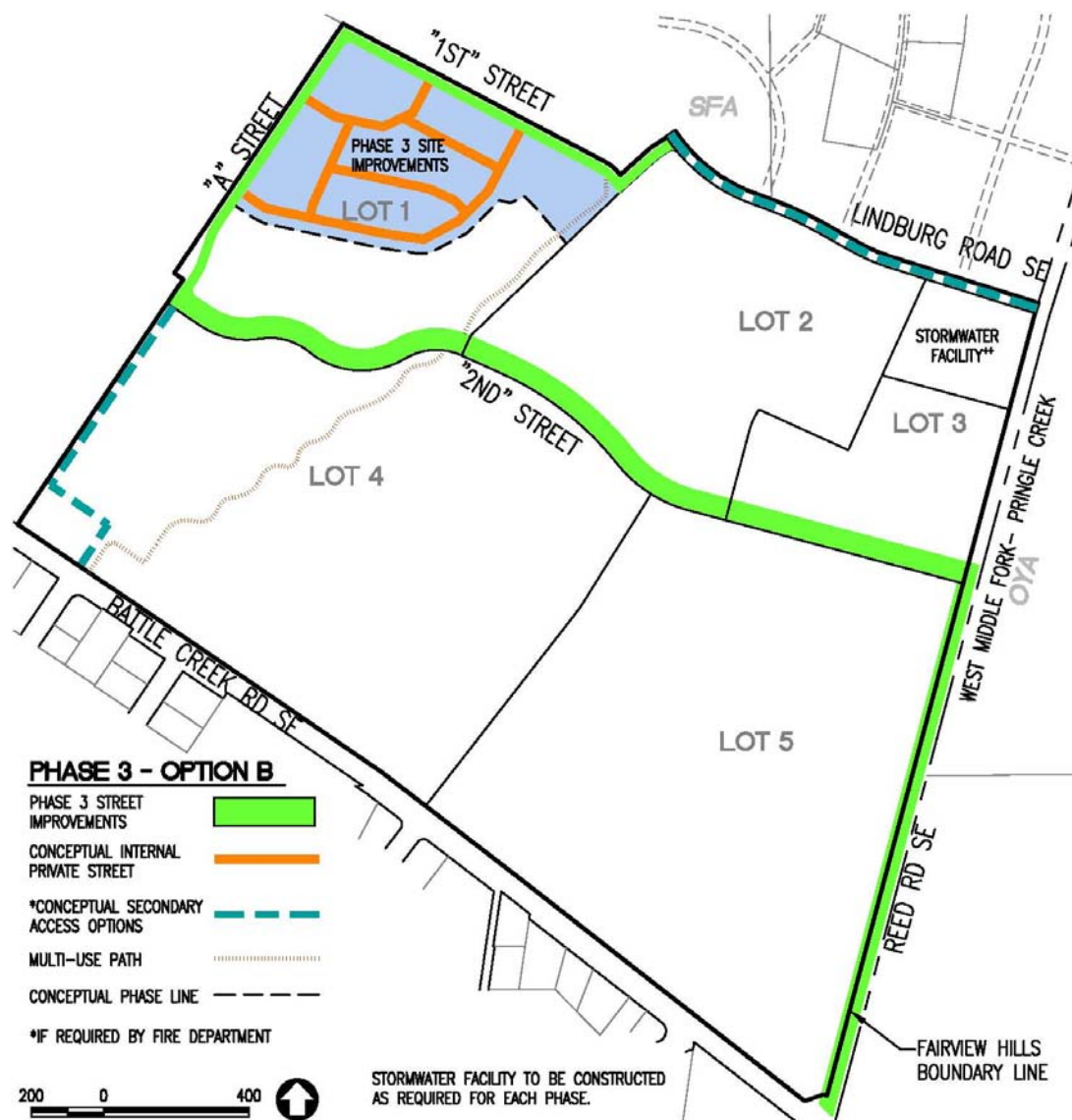
- “2nd” Street improvement (30 ft. wide linking street) from the east lot line to Reed Road SE.
- Phase 1 only - requires deferral or a fee-in-lieu payment for a 1/2 street improvement to Lindburg Road SE along the lot frontage.
- Construction of the multi-use path from the lot to Battle Creek Road SE.
- Phase 2 only - requires 3/4 street improvement to “2nd” Street for lot frontage.



**Figure 29: Street Phasing for Lot 2 (Phases 1 and 2 – Option B)**

### Phase 3 (Lot 1)

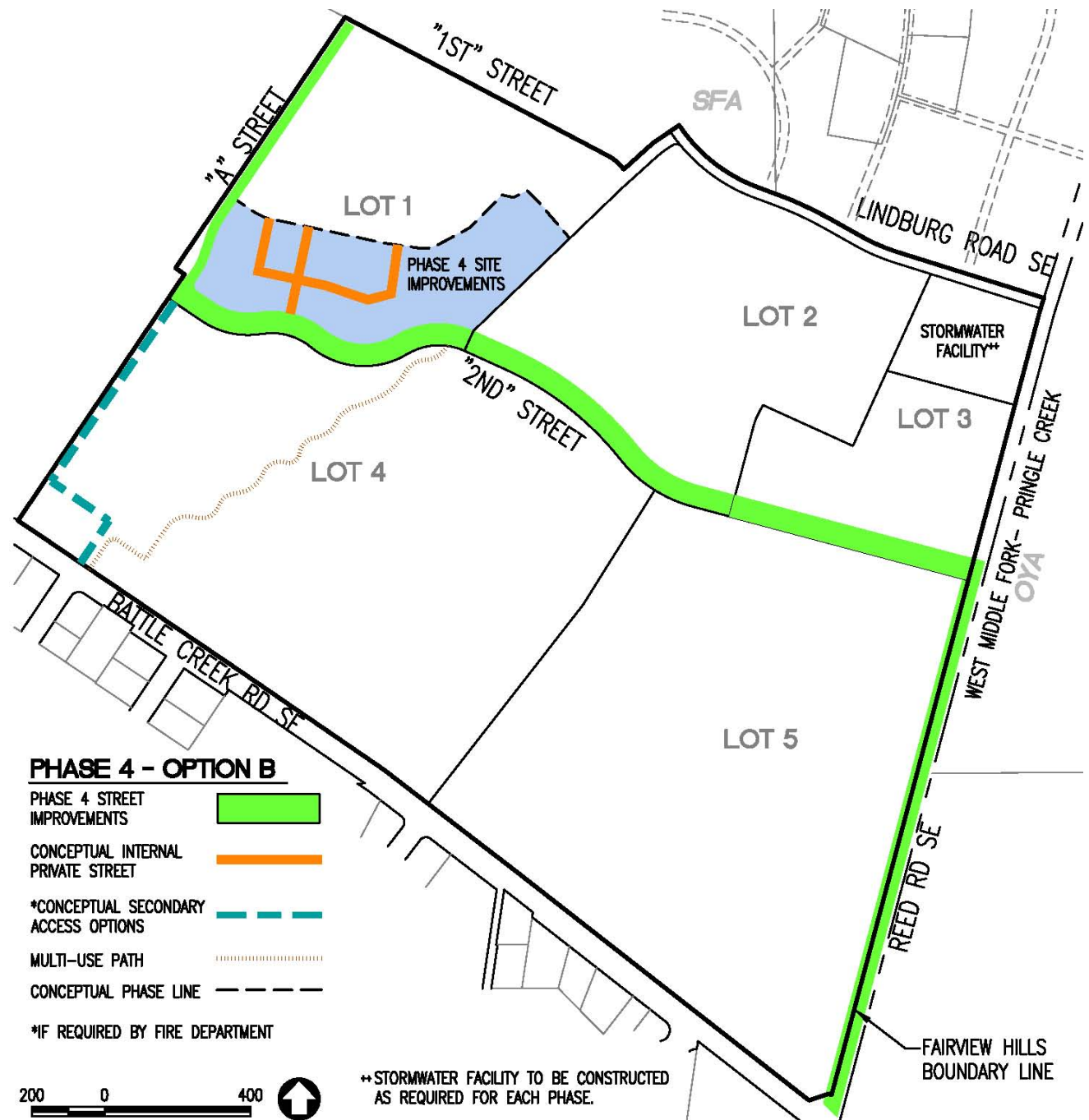
- “2nd” Street improvement (30 ft. wide linking street) from “A” Street to Reed Road SE.
- Deferral or a fee-in-lieu payment for a 1/2 street improvement to Lindburg Road SE along the lot frontage.
- Full improvement to lot frontage on “1st” Street with sidewalk on south side only.
- Full improvement of “A” Street with sidewalk on east side only from “1st” Street to “2nd” Street.
- Construction of the multi-use path from the lot to Battle Creek Road SE.



**Figure 30: Street Phasing for Lot 1 (Phase 3 – Option B)**

Phase 4 (Lot 1)

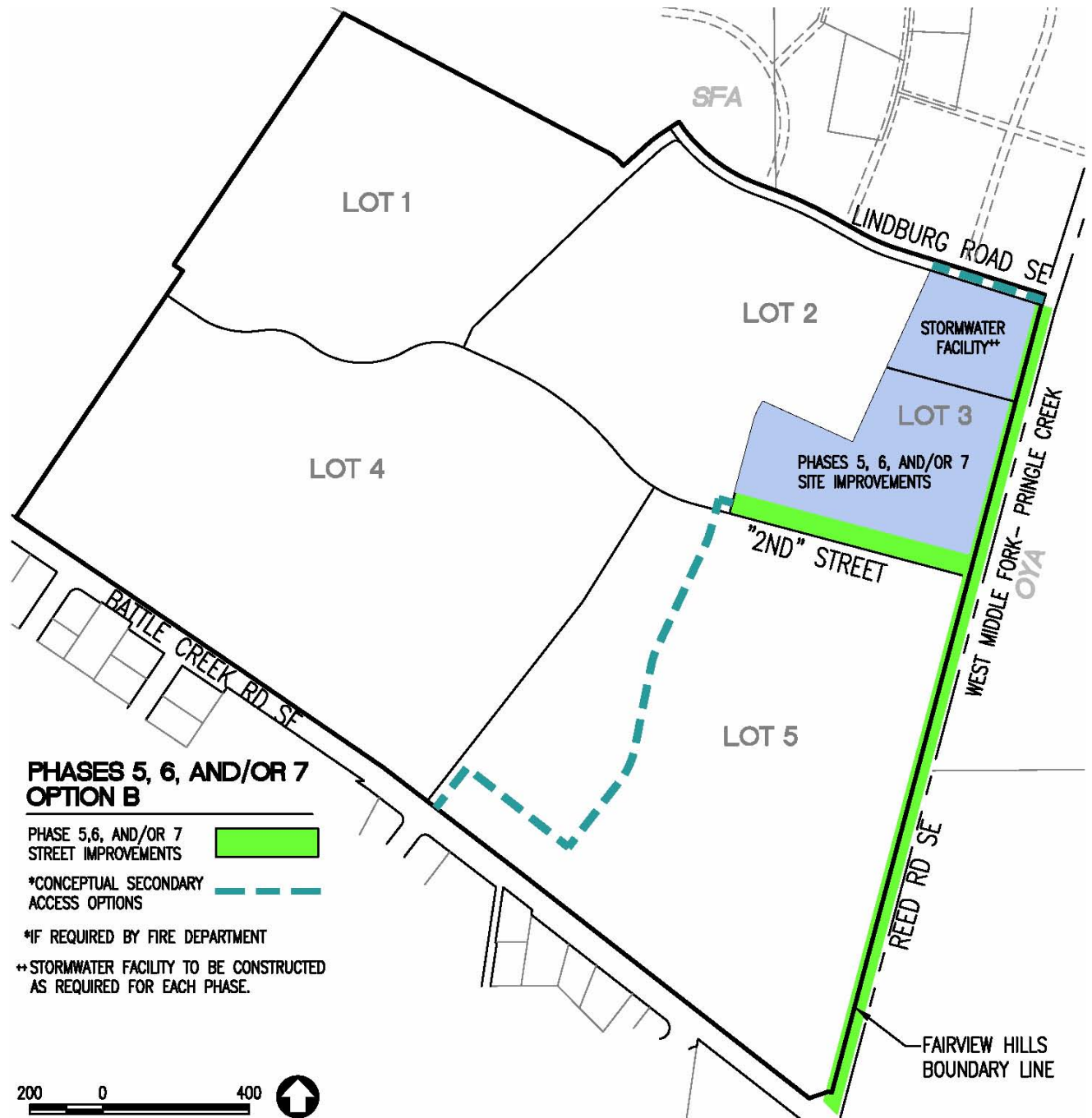
- “2nd” Street improvement (30 ft. wide linking street) from east property line to Reed Road SE.
- 3/4 street improvement to “2nd” Street for lot frontage.
- Full improvement of lot frontage on “A” Street with sidewalk on east side only.
- Construction of the multi-use path from the lot to Battle Creek Road SE.



**Figure 31: Street Phasing for Lot 1 (Phase 4 – Option B)**

Phases 5, 6, and/or 7 (Lot 3)

- Deferral or a fee-in-lieu payment for a 1/2 street improvement to Lindburg Road SE along the lot frontage.
- 3/4 street improvement to lot frontage on Reed Road SE.
- 3/4 street improvement to lot frontage on "2nd" Street.



**Figure 32: Street Phasing for Lot 3 (Phases 5, 6 &/or 7 – Option B)**

Subdivision lots 4 and 5 are outside of the refinement area. The street improvements required as a condition of development on Lots 4 and 5 is discussed below to outline how the City's Preliminary Declaration for Urban Growth Area Development Permit No. 04-8 (UGA) permit conditions of approval are being implemented and to provide a more complete picture of future infrastructure improvements in the area.

#### Lot 4 (Outside of Refinement Plan area)

- *Linking Street*  
None.
- *Boundary Street*  
Construct up to 3/4 street improvement along property frontage on Battle Creek Road SE.
- *Internal Street*  
Full street improvements with all internal streets shown on future plans, unless a street is planned along the west property boundary as that improvement may be completed in cooperation with abutting property owner.

#### Lot 5 (Outside of Refinement Plan area)

- *Linking Street*  
None.
- *Boundary Street*  
Construct up to a 3/4 street improvement along all property frontages on Battle Creek Road SE and Reed Road SE.
- *Internal Street*  
Full street improvements with all internal streets shown on future plans.

## **STANDARDS FOR PHASING OF PUBLIC UTILITIES (12)**

Construction of sanitary sewer, storm drainage, garbage disposal, and public utilities will be completed as necessary to fully serve each development phase and be installed to anticipate future development within the refinement area and adjacent properties. Each phase of development in the refinement area will provide the utility extensions and streets stubs necessary to serve adjacent phases in accordance with the concepts shown in this Refinement Plan, unless modified by subsequent land use planning actions.

## PHASING SCHEDULE (13)

Development within the Refinement Plan area is anticipated to proceed in phases as market conditions allow. The phases are not intended to indicate the order in which the property will develop. A conceptual phasing schedule is shown and summarized below to provide a logical explanation of how development could occur for the Refinement Plan area. However the phasing could change substantially depending on when development on each lot occurs.

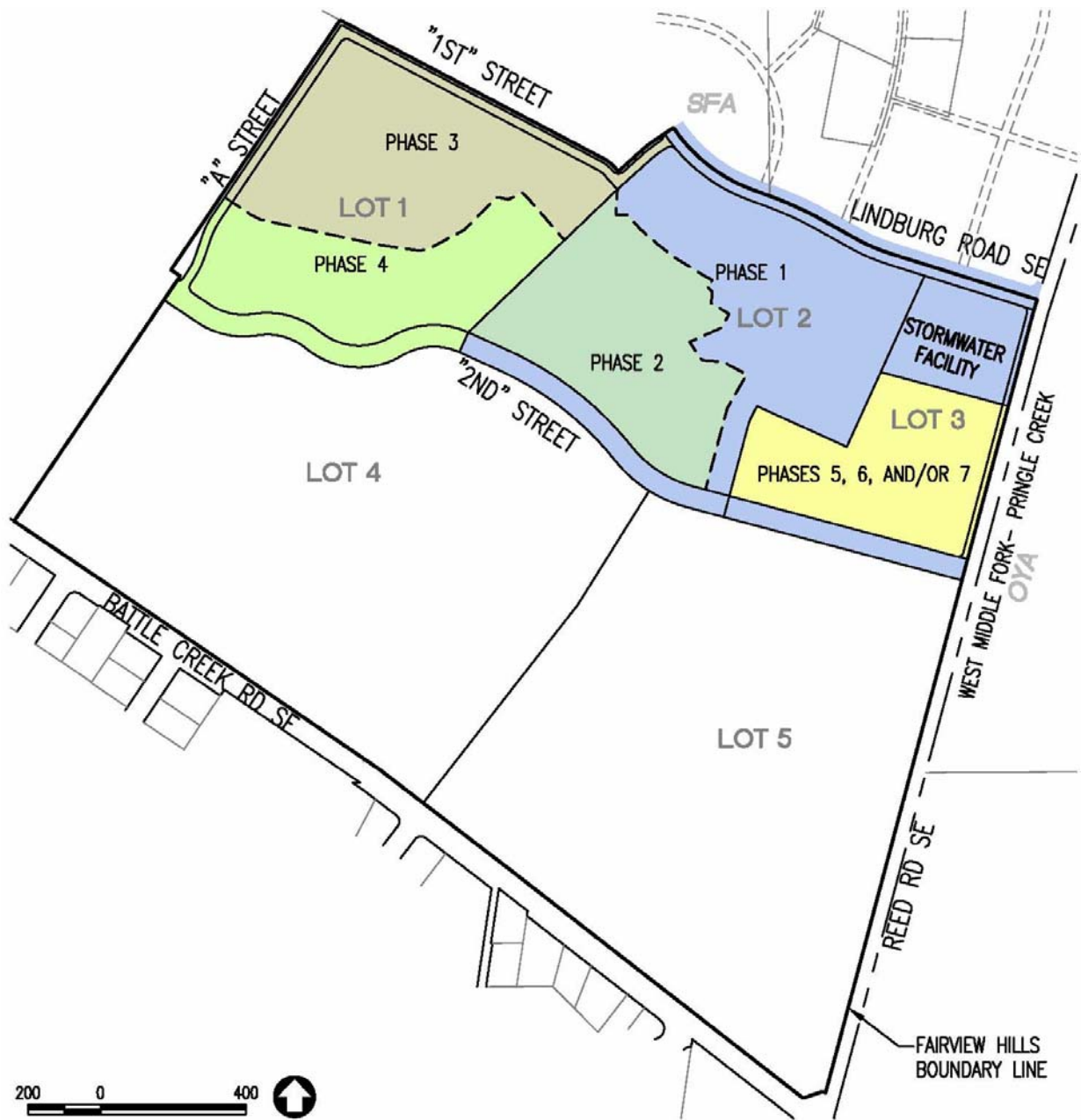


Figure 33: Conceptual Phasing Plan

### **Phase 1 (Lot 2)**

Phase 1 would consist of the first half of apartments planned for Lot 2 including the recreation center, the stormwater facility on Lot 3 as required to support development of Lot 2, and associated infrastructure improvements explained in Section (11) above.

The first phase will preserve the existing wetlands on the south side of Lindburg Road SE, and existing wetlands and green corridor (drainage) on the west boundary of Lot 2. Phase 1 will also preserve the grove of existing Oregon White Oaks located in the northeast corner of the refinement area adjacent to Lindburg Road SE and Reed Road SE as a gateway to the development in conformance with the adopted Fairview Master Plan.

### **Phase 2 (Lot 2)**

Phase 2 would consist of the second half of the total planned apartment units on Lot 2 to complete the project. Phase 2 infrastructure improvements are explained in Section (11) above.

This phase will preserve the green corridor (drainage) along the west boundary and the existing wetlands located at the southeast corner of Lot 2 near “2nd” Street. The view shed in the area of Phase 2 has been incorporated into the conceptual site plan for the apartments by using the existing topography to allow views to the northeast.

### **Phase 3 (Lot 1)**

Phase 3 includes the first half of apartments planned on Lot 1, possible stormwater facility expansion/modification, and construction of infrastructure improvements as explained in Section (11).

Phase 3 will preserve and expand the green corridor (drainage) along the east boundary of Lot 1. The conceptual site plan for the planned apartments has incorporated the natural topography of this hillside to capture the north and northeast views.

### **Phase 4 (Lot 1)**

Phase 4 would consist of the second half of the total planned apartment. Phase 4 infrastructure improvements are outlined in Section (11).

This phase also preserves and expands the green corridor (drainage) along the east boundary of Lot 1. The conceptual site plan for the planned apartments has incorporated the natural topography of this hillside to capture the north and northeast views.



### **Phase 5, 6 and/or 7 (Lot 3)**

These phases would consist of commercial building(s), parking area(s), possible stormwater facility expansion/modification, supporting features such as landscaping and other amenities, and infrastructure improvements as explained in Section (11). The construction of buildings may occur with all the buildings being constructed in one phase or with any combination of buildings depending on market conditions at the time of development.

The existing wetlands and green corridor (drainage) located on Lot 3 will also be preserved as part of the development. There are no significant view sheds identified within the limits of Lot 3.

Each phase of development will stand alone with regards to streets and utilities. No phase is to be developed without adequate streets and utilities available.

### **FINANCIAL ASSURANCES (14)**

The Fairview Hills property is subject to the 2005 Infrastructure Agreement executed by Sustainable Fairview Associates, LLC and the City of Salem. The Infrastructure Agreement creates a Development District specifying schedules, cost estimates, and financing for infrastructure projects related to the development of the entire former Fairview Training Center property. The Infrastructure Agreement is currently being updated to insure changes in infrastructure project needs are addressed.

### **EXTENT THE REFINEMENT PLAN SUPPLEMENTS & SUPERSEDES ADOPTED CITY REGULATIONS (15)**

Specific standards that supplement and supersede City regulations are contained in this Refinement Plan as outlined previously in the Development Standards for FMU Zones (6). The provisions of the SRC shall apply unless specifically addressed in this Refinement Plan.

### **STANDARDS FOR INTERPRETING THE REFINEMENT PLAN (16)**

Development within the Refinement Plan area will be designed to meet the intent of the adopted Fairview Master Plan and the Fairview Mixed Use (FMU) zone code. The FMU zone, Fairview Master Plan, and this Refinement Plan have land use regulations different from the zoning regulations applicable to other zoning districts and therefore are subject to the hierarchy established under SRC 143C.050.



## DESIGN GUIDELINES AND APPROVAL PROCESS (17)

Future development will be designed to comply with this Refinement Plan, the Salem Revised Code, and Development Design Handbook for multi-family developments, including the modifications stated in this document. The City of Salem will review plans for conformance and issue approvals. This will occur through submittal of subdivision and site plan review applications, subsequent Refinement Plan(s) and other standard City requirements, as is customary and appropriate.

## GENERAL LANDSCAPE PLAN (18)

Critical elements of the general landscape plan include: 1) retention of mature trees in accordance with the City approved tree conservation plan, including any amendments thereto; 2) preservation of existing wetlands; 3) providing opportunities for passive and active recreational activities; and 4) incorporation of Best Management Practices (BMPs) for stormwater facilities. The layouts shown in the Refinement Plan have carefully considered opportunities to protect existing trees and wetlands as well as provide for active and passive recreational activities. Three main landscape types have been included for the Refinement Plan area.

- Natural Open Space with wetlands and drainages. Native vegetation is encouraged within these areas.
- Open Space includes multi-use path and trail areas where active and passive recreational activities are provided.
- Private Landscape areas falling within the limits of private developments.

The multi-family developments will provide Common Open Space within each development as required by the Development Design Handbook Section 2-Multiple Family-Open Space A.2.b (1) (d) (3) is 1,000 S.F. with an additional 250 S.F. for every 20 units. The anticipated Common Open Space for each development is about 4,000 S.F. and will consist of exterior active play areas including children play structures, lawn areas and swimming pool areas. In addition, Private Open Space will be provided for each apartment unit as required by the Development Design Handbook Section 2-Multiple Family-Open Space A.4.b. (1) (a) and (b) or at least 96 S.F. per grade unit in the form of patio and adjacent lawn and at least 48 S.F. at units above grade in the form of a private deck.

Common Open Space as required by the Development Design Handbook Section 2-Multiple Family-Open Space A.2.b (1) (a) and (b) requirement will be at least 30% of the site area. Common open space includes landscaping, walkways, play areas, swimming pools, and other areas which provide visual or recreational amenities for the residents. The amount of this



open space cannot exceed 15% of the open space requirement when located on existing grade slopes with 25% or more grade slope.

Natural Open Space will be provided in the two natural drainage areas located on the property in order to adequately protect this natural resource.

Final delineation of the various types of open and natural spaces will occur during the Site Plan Review and Design Review, if applicable.

Plant species tolerant of soil and hydrologic conditions without ongoing irrigation are preferred for all landscaping within the refinement area. In addition to incorporation of native drought tolerant plant and tree species, private development will be required to comply with the applicable sections of the SRC for landscape standards. The multi-use path between Lots 1 and 2 will include way finding signage. The landscape plan for the area surrounding the Oregon White Oak grove at the northeast corner of the property will be designed to minimize impacts to the existing trees and/or retain in its current natural state.

Street trees will be planted in accordance with SRC Chapter 86 to provide shade tree lined public streets within the limits of the Refinement Plan.

The general landscape plan shown below further illustrates these concepts. Detailed landscape plans will be submitted when required as part of the standard City requirements for development on each lot.





Figure 34: General Conceptual Landscape Plan

## GENERAL DRAINAGE PLAN (19)

There are three main ridges that define the drainage sub-basins onsite. Existing off-site uphill drainage from basins 2U and 3U currently flowing into the existing drainages on the Fairview Hills property will continue to flow downstream undetained.

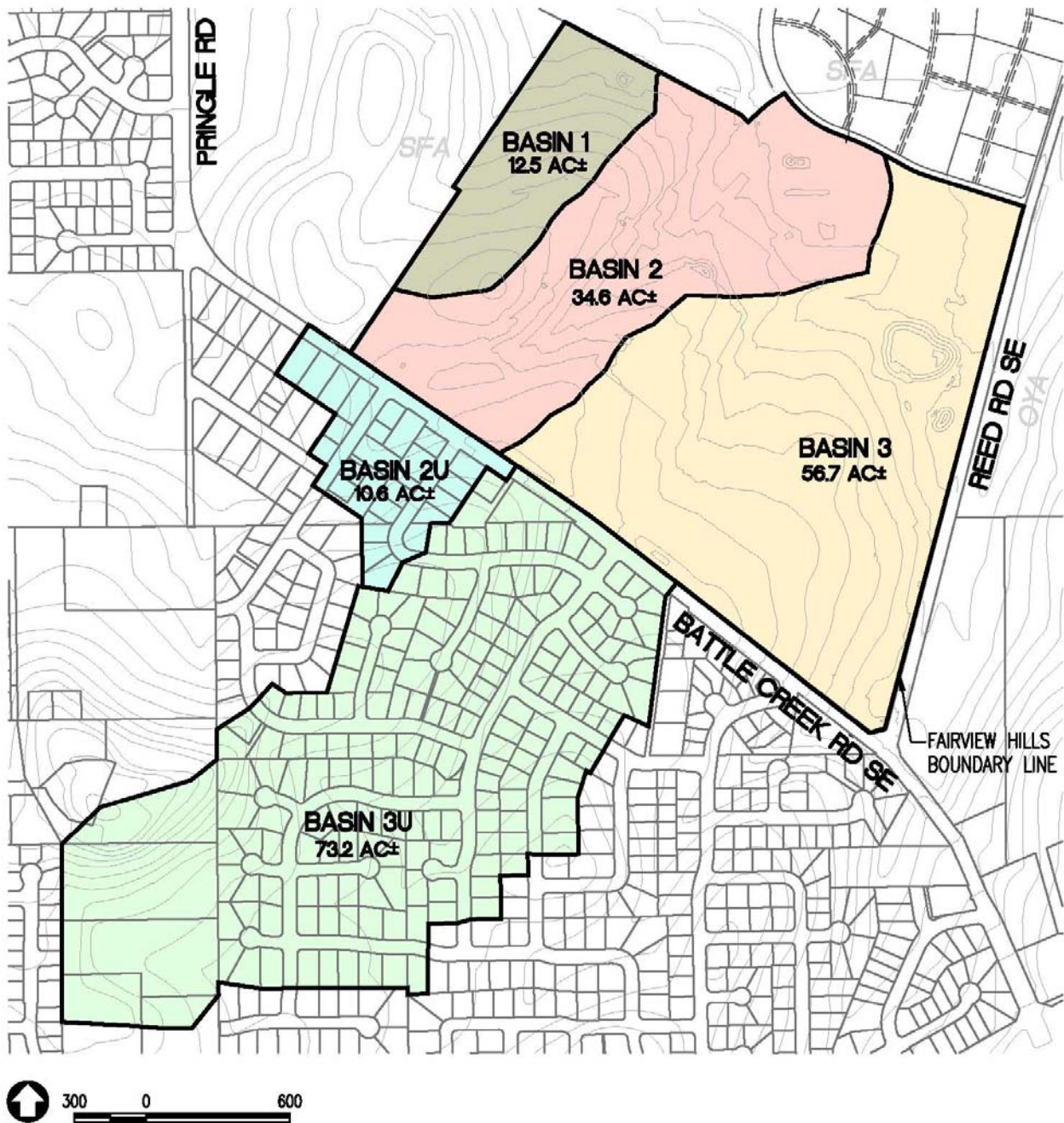


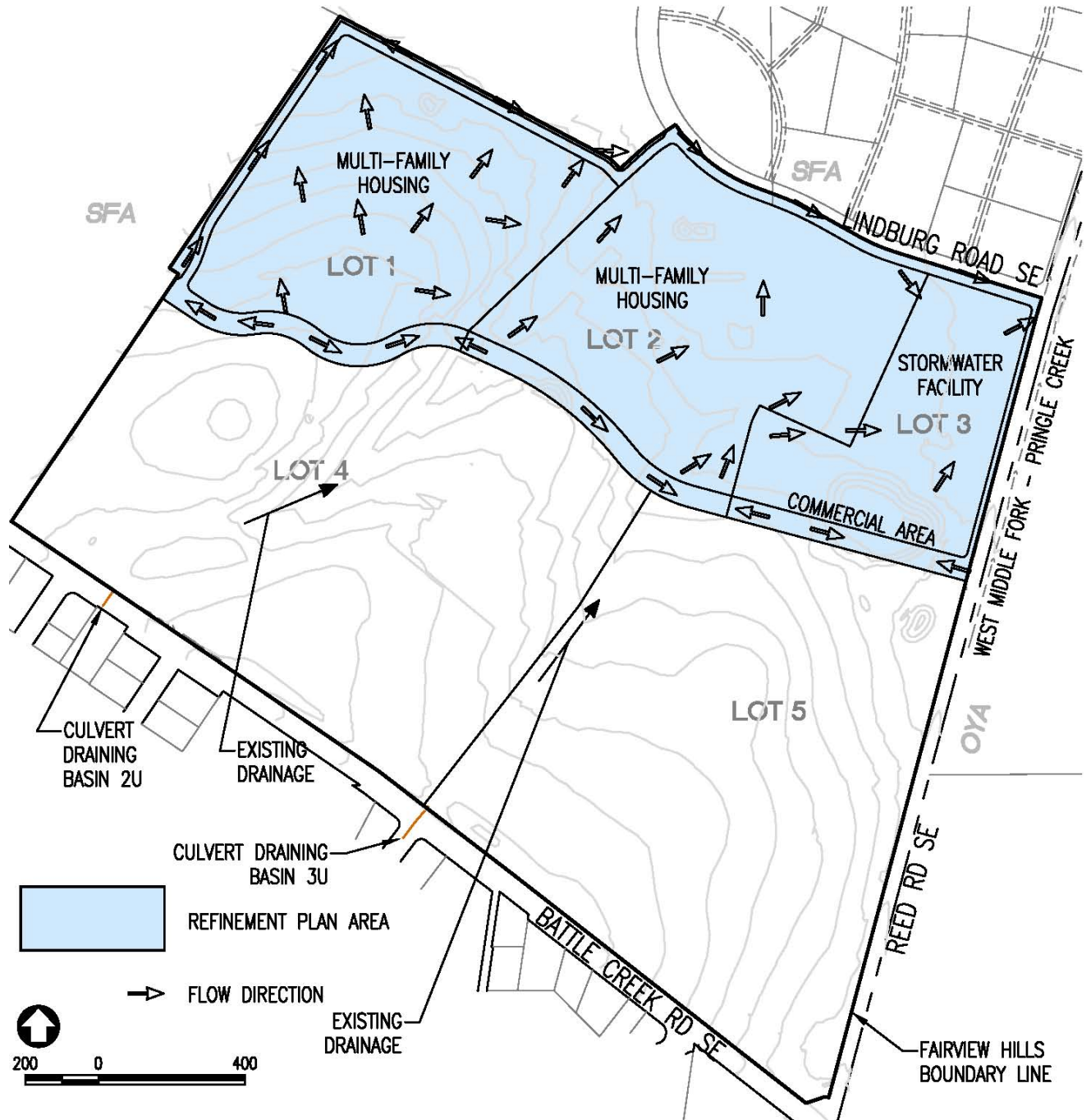
Figure 35: Existing Drainage Basins

The City of Salem has not adopted stormwater quality standards as of the date of this Refinement Plan. Therefore, stormwater quality treatment of runoff from impervious surfaces will be provided for all development within the Refinement Plan area (41.1± acres) and the remaining 62.7± acres of the Fairview Hills property located outside of the Refinement Plan area in conformance with the applicable policies and procedures of Clean Water Services of Washington County (CWS) Design and Construction Standards (R&O 07-20) as of the date of this Refinement Plan. This is an example of the innovative solutions being implemented throughout this Refinement Plan.

Section II. B. of the City of Salem Public Works Department Design Standards for Stormwater Management dated March 16, 2007 states that as interim design standards for stormwater quality the City will accept stormwater quality facilities designed in accordance with Clean Water Services (CWS) standards. Stormwater runoff originating on the Fairview Hills property will be detained in conformance with the City of Salem Public Works Department Design Standards for Stormwater Management dated March 16, 2007 and in effect as of the date of this Refinement Plan.

A public extended dry detention basin is planned for the northeast corner of the refinement area (Lot 3) to serve public right-of-ways as well as private development areas for the full build-out of the property currently owned by Simpson Hills, LLC (103.8± acres). The basin will be designed and constructed in accordance with CWS standards to provide stormwater quality treatment and stormwater quantity management (detention) per City of Salem standards as approved by the Public Works Department. The stormwater discharge location is planned as a new public storm drain outfall to the West Middle Fork Pringle Creek in the proximity of the Lindburg Road SE and Reed Road intersection.





**Figure 36: General Conceptual Drainage Plan**

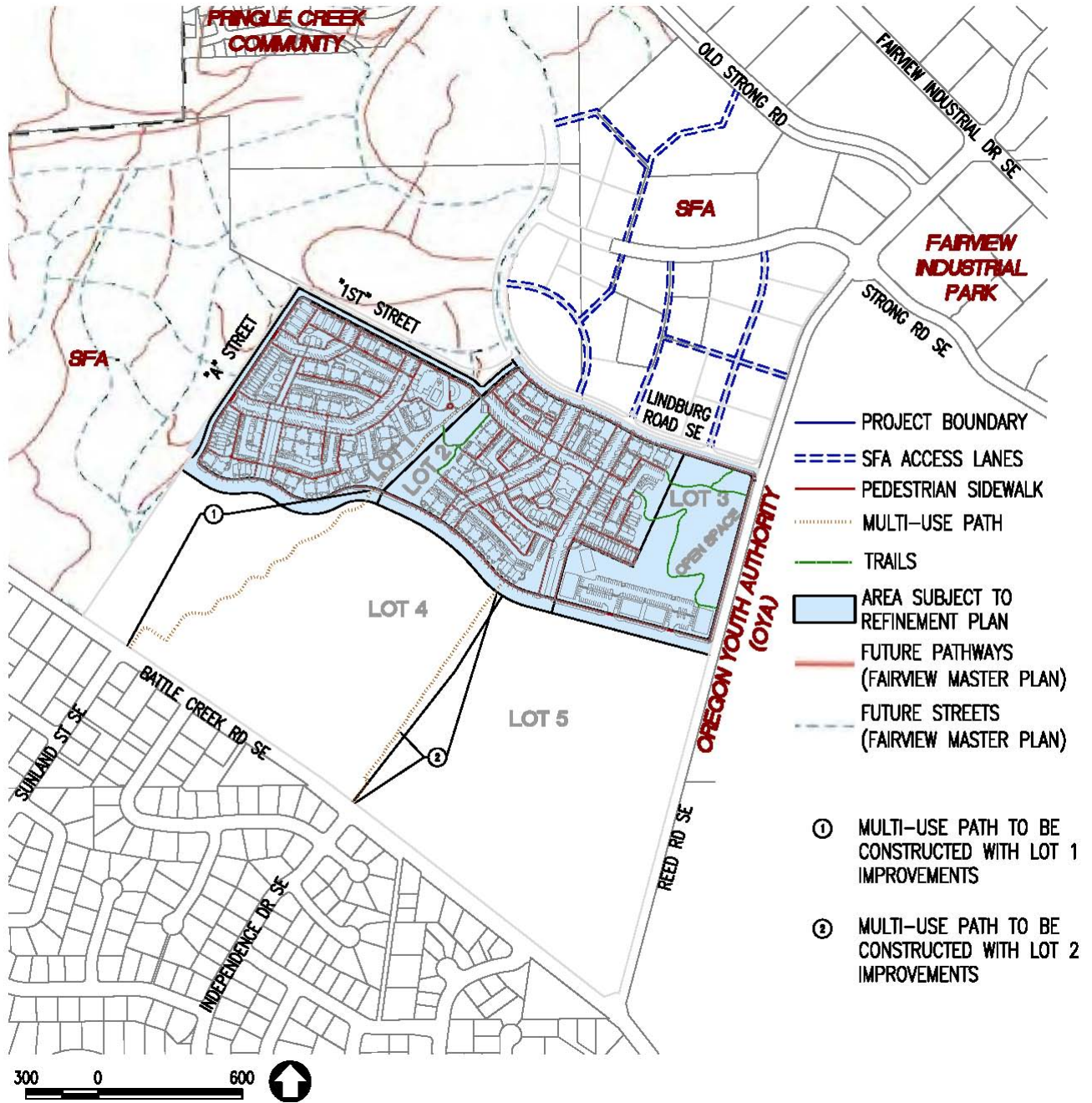
## **TRAFFIC IMPACT ANALYSIS REPORT (20)**

The Fairview Master Plan Traffic Impact Analysis (TIA) estimated a total of approximately 17,000 trips upon full development of the former Fairview Training Center site (275± acres). Subsequent to the Fairview Plan TIA, Kittelson & Associates, Inc. (K&A) prepared a memorandum dated July 22, 2009 providing revised trip generation estimates covering the existing Pringle Creek Community, Fairview Hills property, and Sustainable Fairview Associates, LLC property. The July 22, 2009 K&A memorandum reduced the traffic impact to approximately 12,000 trips. Subsequently, K&A prepared another update on February 7, 2012 based on the Fairview Hills Refinement Plan which indicates that approximately 3,390 new daily trips would be generated if the refinement area, upon full development, matches the conceptual layouts shown. For further information a copy of the K&A memorandum has been included in the Appendix.

## **IMPACTS ON EXISTING STRUCTURES & OTHER DEVELOPMENT (21)**

The Refinement Plan area is bordered on the north and west by Sustainable Fairview Associates (SFA) which is subject to SRC 143C FMU zoning; on the east by Hillcrest Youth Correctional Facility operated by the Oregon Youth Authority (OYA); and on the south by existing single-family residences. Multi-use pedestrian/bike pathways through the property are important amenities that will increase recreational opportunities in the neighborhood. The multi-use paths will coordinate with the Fairview Refinement Plan II. Future SFA refinement plans and Fairview Hills refinement plans will be required to coordinate with all City approved refinement plans and the general intent of the adopted Fairview Master Plan.





**Figure 37: Conceptual Pedestrian Connectivity, Multi-Use Pathway, & Trail Plan**

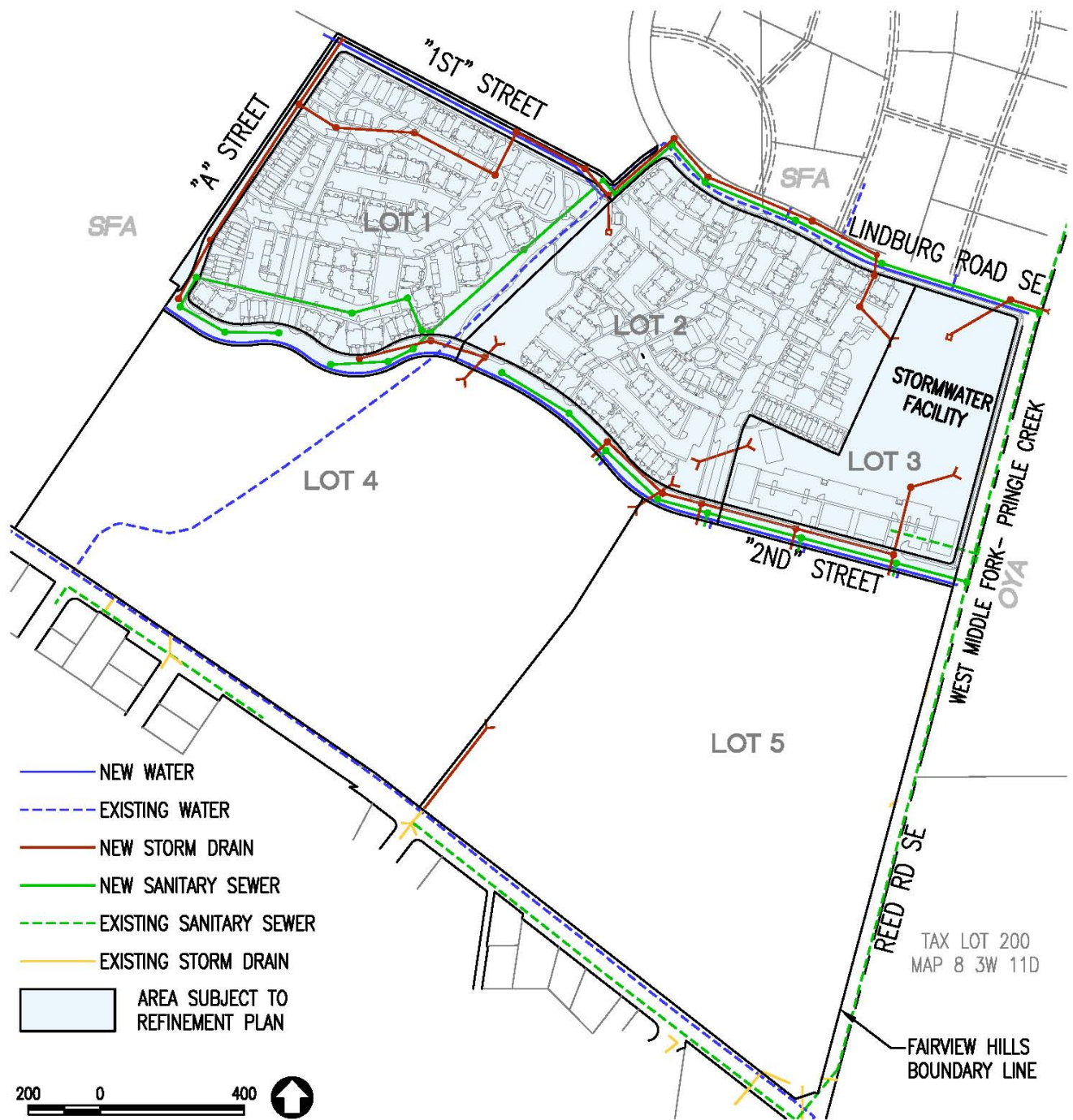
## **IMPACTS ON EXISTING INFRASTRUCTURE & PUBLIC SERVICES (22)**

The Urban Growth Area (UGA) Development Permit (No. 04-8) coupled with the Infrastructure Agreement (IA) entered into by Sustainable Fairview Associates, LLC and the City of Salem on September 20, 2005 identify the infrastructure improvement needs required due to the impacts on the existing infrastructure and public services that the development of the former Fairview Training Center site will create. The IA provides for the method of financing the infrastructure improvements as well as the timing for construction based on triggers due to traffic impacts (K&A Memo) as the former Fairview Training Center property develops. The refinement area shows development that is consistent with the adopted Fairview Master Plan and the impacts to the off-site infrastructure as outlined in the IA.

## **LOCATION AND EXTENT OF SANITARY SEWER, STORM DRAINAGE AND UTILITIES (23)**

Water, sanitary sewer, and storm drain main lines will be public and constructed within street right-of-ways or easements as required by City standards. Separate public water, sanitary sewer and storm drain easements outside of the right-of-ways may be necessary to serve proposed development. Water, sanitary sewer, and storm drain lines will be sized in accordance with City of Salem design standards. Power, telephone, gas and cable lines will be installed within public utility easements following the alignment of several of the public street right-of-ways.

Private sanitary sewer, domestic water, irrigation, fire, storm drains and utilities will be constructed with development of each lot within the Refinement Plan area. Final designs will be completed with construction plans for development of each lot and submitted to the City of Salem for review and permitting as required.



**Figure 38: General Public Utilities Conceptual Plan**

## EXISTING “HISTORICALLY SIGNIFICANT” RESOURCES (24)

The adopted Fairview Master Plan inventory does not identify buildings, structures or sites, which possess the criteria for historic resource designation under SRC Chapter 120A.040 or which have been designated as “historically significant”, as being located within the Refinement Plan area.

As stated in Section 7, there are several potential cultural sites identified in the Archaeological Cultural Resources Inventory & Assessment (Exhibit 6) of the Fairview Master Plan dated August 2004, as well as Appendix ‘A’ dated June 2004, which have been shown on Figure 23. In order to more definitely determine the presence of any cultural resources, two extensive cultural resource investigations that focused on the 103.8± acre Fairview Hills project site were conducted including background research, pedestrian surveys, the excavation of 170 shovel test pits, and the use of a magnetometer to locate evidence for historic-era burials. The report makes the following statements.

“No evidence for burials or a cemetery was uncovered during the fieldwork.”

*“Despite intensive efforts to locate archaeological deposits, combined results of AAR’s surveys suggest that the development area contains very sparse archaeological remains...”*

Copies of the following reports have been included in the Appendix.

- May 6, 2009 AAR Cultural Resources Survey summary
- June 8, 2009 AAR Archaeological Survey
- June 16, 2009 SHPO letter
- July 1, 2009 AAR Archaeological Monitoring Results summary
- September 9, 2010 AAR investigation